

AGENDA CITY OF WATSONVILLE PLANNING COMMISSION MEETING

Motto: "Opportunity through Diversity; Unity through Cooperation."



Mission Statement: "The City of Watsonville is dedicated to improving the economic vitality, safety & living environment for the culturally rich Watsonville community, by providing leadership for the achievement of community goals & high quality, responsive public services."

Planning Commission Members

Mireya Gomez-Contreras, District 1 (Vice-Chair)

Aurelio Gonzalez, District 2

Dobie Jenkins, District 3

Pedro Castillo, District 4

Jenny Sarmiento, District 5

Marty Corley, District 6 (Chair)

Rick Danna, District 7

Marcela Tavantzis, Secretary to Planning Commission

Alan J. Smith, City Attorney

Deborah Muniz, Recording Secretary

**CITY PLAZA COUNCIL CHAMBERS
275 MAIN STREET, 4th FLOOR, 6th LEVEL PARKING
WATSONVILLE, CALIFORNIA**

TRANSLATION SERVICES/ SERVICIOS DE TRADUCCION
Spanish language translation is available/ Traducciones en español disponibles

Americans with Disabilities Act



The Council Chambers is an accessible facility. If you wish to attend a meeting and you will require assistance in order to attend and/or participate, please call the City Clerk's Office at (831) 768-3040 at least five (5) days in advance of the meeting to make arrangements. The City of Watsonville TDD number is (831) 763-4075.

Meetings are televised live on Charter Cable Communications Channel 70 and AT&T Channel 99.

For information regarding this agenda, please call the Community Development Department at (831) 768-3050.

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**AGENDA
CITY OF WATSONVILLE
PLANNING COMMISSION MEETING**

Tuesday, September 2, 2014
6:00 P.M.

Council Chambers
275 Main Street, 4th Floor

Anyone wishing to address the Planning Commission is asked to fill out a yellow speaker card and place it in the box at the podium for recording purposes. Speakers will be limited to three (3) minutes.

IF YOU CHALLENGE ANY ACTION APPEARING ON THIS AGENDA IN COURT, YOU MAY BE LIMITED TO RAISING ONLY THOSE ISSUES YOU OR SOMEONE ELSE RAISED AT THE PUBLIC MEETING DESCRIBED ON THIS AGENDA, OR IN WRITTEN CORRESPONDENCE DELIVERED TO THE CITY CLERK PRIOR TO, OR AT, THE PUBLIC MEETING.

(NOTE: In accordance with City policy, all Planning Commission meetings are recorded in their entirety and are available for review. The minutes are a brief summary of action taken.)

1.0 ROLL CALL OF COMMISSIONERS

2.0 PLEDGE OF ALLEGIANCE (Led by Commissioner Gonzalez)

3.0 PETITIONS AND ORAL COMMUNICATIONS

3.1 ORAL COMMUNICATIONS FROM GENERAL PUBLIC AND PLANNING COMMISSION

(This time is set aside for members of the general public to address the Planning Commission on any item not on the Agenda, which is within the subject matter jurisdiction of the Planning Commission. No action or discussion shall be taken on any item presented except that any Planning Commissioner may respond to statements made or questions asked, or may ask questions for clarification. All matters of an administrative nature will be referred to staff. All matters relating to the Planning Commission will be noted in the minutes and may be scheduled for discussion at a future meeting or referred to staff for clarification and report. Any Planning Commissioner may place matters brought up under Oral Communications on a future agenda.

(ALL SPEAKERS ARE ASKED TO FILL OUT A YELLOW SPEAKER CARD AND PLACE IT IN THE BOX AT THE PODIUM, GOT TO THE PODIUM AND ANNOUNCE THEIR NAME AND ADDRESS IN ORDER TO OBTAIN AN ACCURATE RECORD FOR THE MINUTES OF THE MEETING.)

4.0 CONSENT AGENDA

All items appearing on the Consent Agenda are recommended actions which are considered to be routine and will be acted upon as one consensus motion. All items removed will be considered immediately after the consensus motion. The Chairman will allow public input prior to the approval of the Consent Agenda.

4.1 MOTION APPROVING MINUTES FOR THE JULY 1, 2014 REGULAR MEETING

5.0. NEW BUSINESS

**5.1 PRESENTATION ON DOWNTOWN MAIN STREET IMPROVEMENT PROJECT
(10 minutes)**

6.0 PUBLIC HEARINGS

6.1 A PUBLIC HEARING TO CONSIDER AN APPLICATION FOR A MINOR LAND DIVISION, SPECIAL USE AND DESIGN REVIEW PERMIT WITH ENVIRONMENTAL REVIEW (PP2014-165), TO CREATE A 10,193 SQ. FT. LOT TO ALLOW THE CONSTRUCTION OF A TWO STORY 8,973 SQ. FT. AUTO PARTS STORE, AT 17 WEST LAKE AVENUE (APN:



017-641-09), FILED BY MITCH BRAMLITT OF AUTOZONE CORPORATION, APPLICANT, ON BEHALF OF WILLIAM HANSEN, PROPERTY OWNER.

- a) Staff Presentation (5 minutes)
- b) Applicant Presentation (5 minutes)
- c) Public Hearing
- d) Commission Discussion
- e) Staff Recommendation: Adopt a resolution approving Application (PP2014-165).

6.2 A PUBLIC HEARING TO CONSIDER RECOMMENDATION TO THE CITY COUNCIL ON APPLICATION (PP2014-175) FOR A REZONING FROM INSTITUTIONAL ZONING (N) TO CENTRAL COMMERCIAL (CC), FOR PARCEL (APN: 017-183-04) WITH CONSIDERATION OF A TEXT AMENDMENT TO (WMC 14-16.1104 (B)), FOR ALLOWANCE OF A DRIVE-THROUGH FACILITY WITH A USE PERMIT IN THE CC ZONE. ADDITIONALLY, CONSIDER RECOMMENDATION ON APPLICATION (PP2014-117), FOR A SPECIAL USE PERMIT WITH DESIGN REVIEW AND ENVIRONMENTAL REVIEW, TO CONSTRUCT A 4,278 SQ. FT. 24 HOUR FAST FOOD RESTAURANT WITH A DRIVE-THROUGH FACILITY (MCDONALD'S); INCLUDING A VARIANCE TO REDUCE THE DISTANCE OF THE DRIVE-THROUGH FACILITY FROM 200 FEET TO 160 FEET (WMC 14.41.100 (A) (2)), FROM A SIGNALIZED INTERSECTION SUBJECT TO CITY COUNCIL ACTION ON THE REZONING AND TEXT AMENDMENT, AT 174,186, AND 190 MAIN STREET (APNS: 017-183-04, 05, & 10), FILED BY CORE STATES GROUP, APPLICANT, ON BEHALF OF FRED ODA, LUPE BELTRAN AND THE CITY OF WATSONVILLE, PROPERTY OWNERS.

- a) Staff Presentation (5 minutes)
- b) Applicant Presentation (5 minutes)
- c) Public Hearing
- d) Commission Discussion
- e) Staff Recommendation: Adopt a resolution recommending to the City Council approving Applications (PP2014-175) and (PP2014-117).

7.0 REPORT OF SECRETARY

8.0 ADJOURNMENT

The next meeting is scheduled for **Tuesday, October 7, 2014 at 4:30 p.m.** in the City Council Chambers, 275 Main Street, 4th Floor (6th level parking), Watsonville, California.

Pursuant to Section 549.54.2(a)(1) of the Government Code of the State of California, this agenda was posted at least 72 hours in advance of the scheduled meeting at a public place freely accessible to the public 24 hours a day.

Materials related to an item on this Agenda submitted to the Planning Commission after distribution of the agenda packet are available for public inspection in the Community Development Department (250 Main Street) during normal business hours. Such documents are also available on the City of Watsonville website at www.cityofwatsonville.org subject to staff's ability to post the document before the meeting.



MINUTES

REGULAR MEETING OF THE PLANNING COMMISSION
OF THE CITY OF WATSONVILLECOUNCIL CHAMBERS
275 MAIN STREET, 4th FLOOR, WATSONVILLE, CALIFORNIA

Tuesday, July 1, 2014

6:07 P.M.

In accordance with City policy, all Planning Commission meetings are recorded on audio and videotapes in their entirety, and the tapes are available for review in the Community Development Department (CDD). These minutes are a brief summary of action taken.

1.0 ROLL CALL OF COMMISSIONERS

Present were Commissioners, Rick Danna, Aurelio Gonzalez, Dobie Jenkins, Jenny Sarmiento (arrived at 6:11 p.m.), and Chair Marty Corley. Commissioner Pedro Castillo and Vice-Chair Mireya Gomez-Contreras were absent.

Staff members present were Secretary Marcela Tavantzis, Senior Planner Suzi Merriam, Recording Secretary Angela Paz and City Interpreter Carlos Landaberry.

2.0 PLEDGE OF ALLEGIANCE

Commissioner Danna led the Pledge of Allegiance.

3.0 PETITIONS AND ORAL COMMUNICATIONS:

Commissioner Jenkins asked if the City was going to sell Buena Vista Park and if this was something that was brought to the Planning Commission for approval.

Secretary Tavantzis stated that yes, the park will be sold and that the item went directly to City Council.

4.0 CONSENT AGENDA**4.1 MOTION APPROVING MINUTES FOR THE MAY 6, 2014 AND THE MAY 20, 2014
REGULAR MEETINGS**

MOTION: It was moved by Commissioner Danna, seconded by Commissioner Gonzalez, and carried by the following vote to approve the Consent Agenda:

AYES:	COMMISSIONERS:	Danna, Gonzalez Jenkins, Corley
NOES:	COMMISSIONERS:	None
ABSENT:	COMMISSIONERS:	Castillo, Gomez-Contreras, Sarmiento
ABSTAIN:	COMMISSIONERS:	None

5.0 PUBLIC HEARINGS**5.1 A PUBLIC HEARING TO CONSIDER AN APPLICATION FOR A SPECIAL USE
PERMIT (PP2014-135) TO ALLOW THE ESTABLISHMENT OF A FENCE**

UNADOPTED MINUTES

CONTRACTOR IN AN EXISTING 1,000 SQ.FT. LEASE SPACE, AT 476 AIRPORT BOULEVARD (APN: 015-131-02), FILED BY RUSSELL JONES, APPLICANT.

a) Staff Presentation

The staff report was given by Senior Planner Suzi Merriam.

Chairperson Corley asked if the Fire Department required fire sprinklers and did they approve the project.

Ms. Merriam stated that fire sprinklers were not required and that the Fire Department had approved the project.

Commissioner Danna inquired about customer flow in the building and how many employees he had at the time.

Mr. Jones stated that he would have very few customers in the office and that at the time, he had no employees.

Commissioner Gonzalez inquired about the types of fences he worked on and if there would be welding in the building.

Mr. Jones stated that he builds all types of fences and that there would be some welding in the building.

Commissioner Sarmiento asked Mr. Jones if he had experience in this line of work, where his business was previously located, and if he was expanding his business.

Mr. Jones stated that he had 10 years of experience in this line of work, that his business was located in Santa Cruz County, and that he is not expanding his business, he is relocating it to Watsonville.

Commissioner Jenkins asked if he would be working with combustible material.

Mr. Jones stated that not likely but if he did, the materials would be stored in a flame proof cabinet.

Commissioner Sarmiento asked the applicant if he was licensed.

Mr. Jones stated that he was a state licensed fence contractor and would also obtain a City business license.

b) Applicant Presentation

Applicant, Russell Jones was present to answer questions.

Chairperson Corley inquired about fire sprinklers in the building.

Mr. Jones stated that the building did not have fire sprinklers.



UNADOPTED MINUTES

c) Public Hearing

Chairperson Corley opened the Public Hearing, and hearing no public comments, closed the Public Hearing.

d) Commission Discussion

None.

e) Motion:

It was moved by Commissioner Danna, seconded by Commissioner Jenkins, and carried by the following vote to adopt a resolution approving Special Use Permit (PP2014-135):

AYES:	COMMISSIONERS:	Danna, Gonzalez, Jenkins, Sarmiento, Corley
NOES:	COMMISSIONERS:	None
ABSENT:	COMMISSIONERS:	Castillo, Gomez-Contreras

6.0 REPORT OF SECRETARY

Secretary Tavantzis informed the Commission that the Climate Action item on the agenda was moved to a future date and passed out a handout with a detailed explanation. She also informed them that the Appleton Grill's 6 months review would be moved to a future date because it has had no incidents in the past 6 months.

Commissioner Jenkins asked if there was a requirement for the number of security guards assigned to special events.

Secretary Tavantzis stated that it was based on occupancy.

Commissioner Gonzalez asked if the Special Use Permit was approved for private events or for the property owner.

Secretary Tavantzis stated that it was for private events.

Commissioner Sarmiento stated that there have been several complaints with the upstairs residents and asked if anything was being done regarding that.

Secretary Tavantzis replied that the City is working with the residential complex management.

Commissioner Jenkins asked if there were any other complaints with other residential complexes and inquired about the approval of the 56 Atkinson Lane project.

Secretary Tavantzis stated that there has been and the City is working with Santa Cruz County Mental Health to alleviate the problems. She indicated that 56 Atkinson Lane project was approved unanimously by City Council.

Chairperson Corley asked if El Miramar Sports Bar was still operating and asked for additional information regarding Manabe-Ow EDA.



UNADOPTED MINUTES

Secretary Tavantzis stated that El Miramar would be in operation until August 2014 and that the main road for Manabe-Ow has been build but that there was more work to be done.

Commissioner Danna inquired about the types of tenants that are being sought for Manabe-Ow.

Secretary Tavantzis stated that Industrial tenants.

Commissioner Jenkins asked about discussions regarding a possible sports complex.

Secretary Tavantzis stated that there have been some discussions and also reminded the Commission that Tuesday, August 5, 2014 is National Night Out.

7.0 ADJOURNMENT

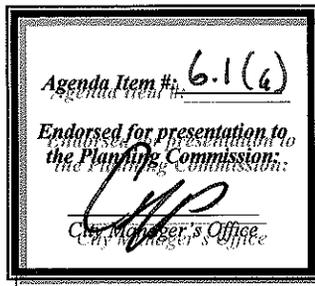
Chairperson Corley adjourned the meeting at 7:41 p.m. The next Planning Commission meeting is scheduled for Tuesday, September 2, 2014, at 4:30 p.m. in the City Council Chambers, 275 Main Street, 4th Floor (6th level parking), Watsonville, California.

Marcela Tavantzis, Secretary
Planning Commission

Marty Corley, Chairperson
Planning Commission



City of Watsonville
MEMORANDUM



DATE: August 5, 2014
TO: Planning Commission
FROM: Marcela Tavantzis, Community Development Director
Keith Boyle, Principal Planner

SUBJECT: SPECIAL USE PERMIT, DESIGN REVIEW, MINOR LAND DIVISION AND ENVIRONMENTAL REVIEW (PP2014-165) TO ALLOW THE DEVELOPMENT OF A TWO STORY 8,973 SQ. FT. AUTOZONE AUTO PARTS RETAIL SALES FACILITY ON A NEW 10,193 SQ. FT. LOT AT 17 WEST LAKE AVENUE (APN: 017-641-09)

AGENDA ITEM: September 2, 2014 Planning Commission

RECOMMENDATION:

Staff recommends that the Planning Commission approve the Special Use Permit, Design Review, Minor Land Division (PP2014-165) and Environmental Review to allow the development of a two story 8,973 sq. ft. Auto Zone auto parts retail sales facility on a new 10,193 sq. ft. parcel at 17 West Lake Avenue, based on the attached findings and conditions of approval.

BASIC PROJECT DATA

APN: 017-641-09 **LOT SIZE:** 10,193+/- square feet

ZONING: CCA (Central Commercial Core Area)

GENERAL PLAN DESIGNATION: Downtown Mixed Use

EXISTING USE: Vacant parking area for Hansen Complex

PROPOSED USE: Two story 8,973 sq. ft. auto parts sales facility on new 10,193 sq. ft. parcel.

SURROUNDING USES: Various commercial uses,

FLOOD ZONE: No (FEMA Panel Number 06087C0393D)

CEQA REVIEW: This project is eligible for a Class 3 Categorical Exemption per Section 15303 of the CEQA regulations as it involves development



of less than 10,000 sq. ft. of commercial space that meets existing zoning regulations.

APPLICANT: Mitch Bramlitt, 123 South Front St., Memphis, TN, 38103

PROPERTY OWNER: William Hansen, 800 East Lake Ave., Watsonville, CA 95076

OVERVIEW:

The applicant, AutoZone Development Corp, has contracted with the property owner, William Hansen, to subdivide a 10,193 sq. ft. lot from an existing private commercial lot for the development of a stand-alone two story 8,973 sq. ft. AutoZone retail sales building, and has submitted a Special Use Permit, Design Review and Minor Land Division application to create those facilities. The site is currently used as a small portion of the private parking lot for Bill Hansen's commercial buildings at 433 Main Street.

PROCESS:

Chapter 14-16-1002 (b) of the Watsonville Municipal Code (WMC) allows for the development of a new retail auto parts building in the Central Commercial Core Zoning district with the approval of a Special Use Permit, and Design Permit by the Planning Commission. Additionally, the applicant has submitted a Minor Land Division application to subdivide the existing 4.3 acre parcel, by separating a 10,193 sq. ft. (.234 acres) parcel from the larger parcel. A public hearing is required for both the Special Use Permit and the Minor Land Division and is being combined as one project for Planning Commission review. Section 14.12-509 of the WMC requires that Use Permit applications be combined with subdivision applications so the hearing can be conducted concurrently.

DISCUSSION:

Existing Site: The proposed project is located on a 4.3 acre parcel that is currently occupied by the vacant 80,000 square foot building previously occupied by Gottschalks, and the County office building, and approximately 290 parking spaces. The application proposes to subdivide a 10,193 portion of the parcel that is currently occupied by 24 parking spaces along the frontage of West Lake Avenue, from the site to accommodate the proposed 8,973 sq. ft. two story building along with 10 parking spaces. Two existing driveways are located off of West Lake Avenue that will not be significantly modified as a result of this project. The site is part of the Downtown Land Use Area which establishes architectural guidelines for new buildings in the downtown. Specifically, the site is in the Main Street Market Place area of the downtown guidelines. Additionally, the site is located in the City's downtown parking district, which does not require additional parking for new commercial buildings due to existing public parking in the area.

Proposed Building: The applicant proposes to construct a two story 8,973 sq. ft. building on the site, with a 4,522 sq. ft. foot print and the remainder of the site for parking, circulation and landscaping. The first floor is intended as the retail space and



the second floor is for storage of auto parts. The two story building design is consistent with the design standards identified in the Main Street Market Place standards of the downtown guidelines. The exterior of the building has been designed with a detailed roof cornice and the second floor has been enhanced with vertical and horizontal banding to break up the massing of the second floor. No windows are proposed for the second floor although the detailing is intended to mimic window placement that is on the bottom floor of the building. The primary entrance of the building will be off the parking lot. This will include windows and entry doors. The East Lake Avenue frontage will be designed with opaque glazing with awnings to mimic a store front but will not function as a real store front due to security reasons. The base of the building will be detailed with a masonry veneer to enhance the building. Awnings, colors and horizontal and vertical detail elements will help break up the building massing and be reflective of the surrounding buildings in the downtown. As designed, the building is consistent with the design guidelines for the Main Street Market Place designation.

Building Setbacks: The project proposes a .5 foot setback from the West Lake Avenue frontage which is consistent with the zero foot allowance in the district. Side yard setbacks will be 5 feet on the north side of the building and 34 feet on the south side of the building both of which are consistent with the CCA standards. The project will replace two existing trees with new 24 inch box jacaranda trees per the landscape plans and include new standard city tree grates.

Proposed Business: The applicant proposes the new two story 8,973 sq. foot retail building to accommodate an AutoZone retail auto parts store. This is an allowed use in the CCA district with a Special Use Permit approved by the Planning Commission per Section 14-16.1002 (b) GLU 553 of the WMC. Staff is supportive of the proposed use as it will complement the area. No disabled vehicles will be allowed on the site. It is anticipated that the project will generate 6 full time jobs and 6 part time jobs at the opening of the business.

Operating Hours: The business is proposed to be open to the public from 8:00 a.m. to 9:00 p.m. seven days per week.

Site Plan/Parking: The site is located within the City's downtown parking district so no additional parking is formally required. However, the WMC requires 5 parking spaces for the first 3,000 sq. ft. of floor area for auto supply stores plus one space for each 800 square feet beyond that. With 8,973 sq. ft. the 10 spaces provided is adequate since the project is within the parking district, although 12 spaces would be required if the site was not in the parking district. As proposed, the parking requirement is accommodated.

Subdivision: The project includes a Tentative Map to subdivide a .23 acre portion of the site from the larger 4.3 acre lot. The lot is proposed to be approximately 128 feet long and 79 feet wide and is proposed between the two existing driveways on the existing lot. The applicant will be required to meet accessibility standards. Any damage to the driveways or existing sidewalks will have to be replaced.



Traffic Study: The City standards do not require preparation of a Traffic Study for commercial buildings less than 10,000 sq. ft. if traffic impacts are not anticipated. Auto parts stores generally do not generate significant traffic impacts and the proposed project only includes 4,500 sq. ft. of retail floor space with the remainder of the area reserved for storage. It is not anticipated that the project will create significant traffic impacts that cannot be addressed by standard traffic impact fees for a commercial project. Therefore no Traffic Study was required for this project.

The site will also be required to address drainage issues required by the Public Works Department. A drainage report has been submitted indicating how that will be achieved. As proposed, the 10,193 sq. ft. lot is consistent with the subdivision standards for lot width, depth and size and will be required to make infrastructure standards meet City requirements for sewer, water, and utility and disabled access. A Soils Report will be required as part of the Building Permit application to determine foundation requirements for the building.

The project is a Minor Land Division so final action on the project can be taken by the Planning Commission for The Minor Land Division, Special Use Permit and Design Review, unless the project is appealed to the City Council.

CONCLUSION:

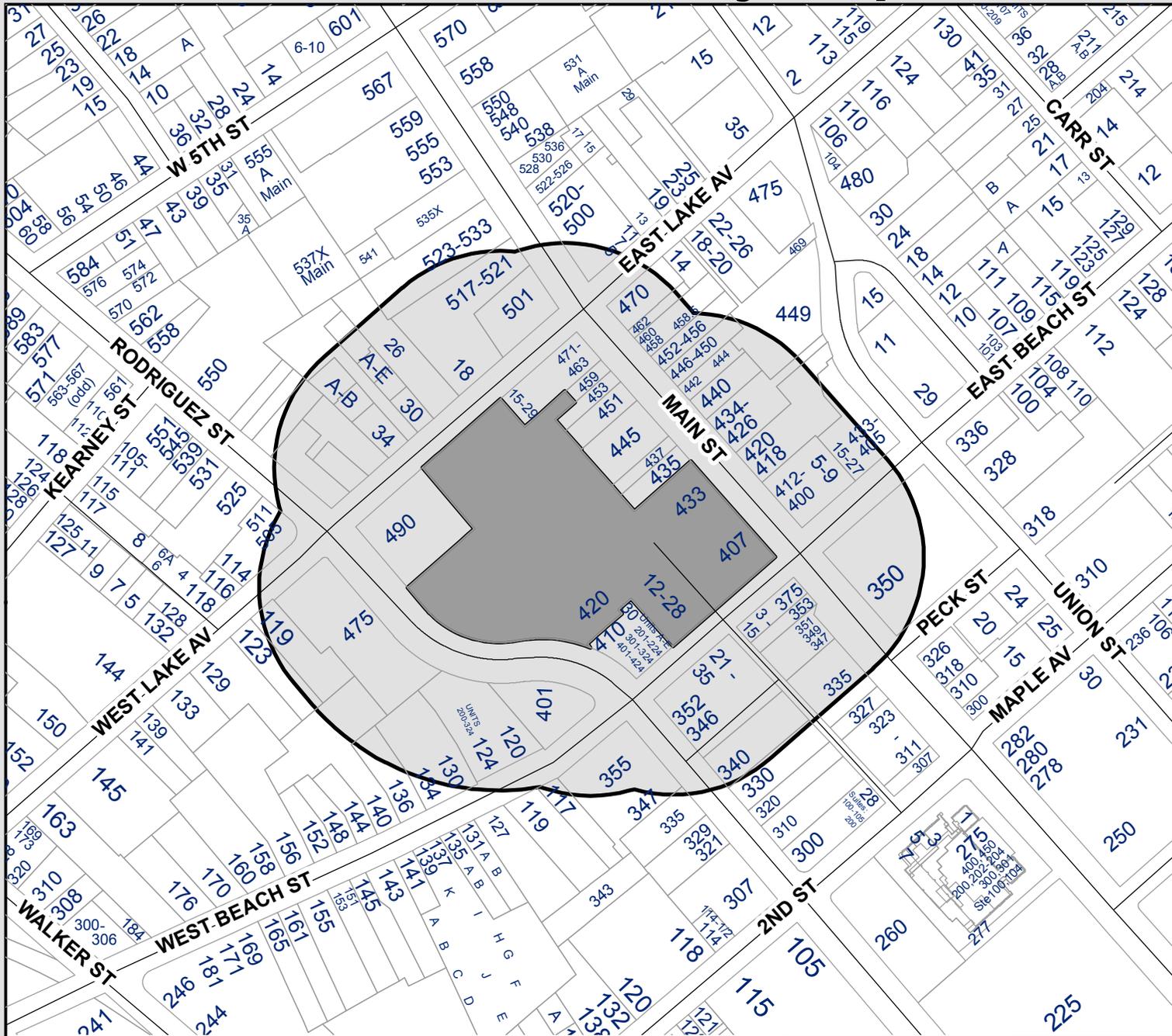
The proposed subdivision and auto parts store on West Lake Avenue meets all requirements of the Central Commercial Core Zoning District and provides sufficient parking for the proposed use. Staff recommends that the Planning Commission approve the project, based on the attached findings and conditions.

ATTACHMENTS:

1. Site and Vicinity Map
2. Project Plans



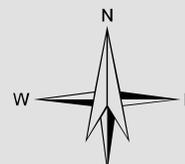
Site and Vicinity Map



Legend

-  Street
-  Parcel
-  300' Buffer Zone
-  Watsonville City Limit
-  Project Site

PROJECT: PP2014-165
 APPLICANT: Mitch Bramlitt
 APN#s: 017-641-09
 LOCATION OF PROJECT: 17 West Lake Ave



ATTACHMENT 1
 Page 1 of 1



Prepared by Watsonville GIS Center 8/05/2014 (CODD1450).

This Document is a graphic representation only of best available sources.
 The City of Watsonville assumes no responsibility for any errors.

LEGAL DESCRIPTION:

BEING A PORTION OF THE CITY OF WATSONVILLE, COUNTY OF SANTA CRUZ STATE OF CALIFORNIA, DESCRIBED AS FOLLOWS:

PARCEL ONE:

BEING A PORTION OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF MAIN STREET WITH THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE BEARING NORTH 41° 10' 00" WEST 250.00 FEET TO THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWO:

BEING A PORTION OF THE LANDS DESCRIBED IN THE DEED FROM CHARLES FORD COMPANY TO RICHARD W. WALKER, DATED SEPTEMBER 29, 1970, IN VOLUME 219 OF DEEDS, PAGE 428, OFFICIAL RECORDS OF SANTA CRUZ COUNTY, THENCE ALONG SAID NORTHWESTERN BOUNDARY...

PARCEL THREE:

BEING A PORTION OF THE LANDS DESCRIBED IN THE DEED TO MARY RESTAR, ET AL., RECORDED IN VOLUME 184, PAGE 304, OFFICIAL RECORDS OF SANTA CRUZ COUNTY, AND BEING MORE PARTICULARLY BOUND AND DESCRIBED AS FOLLOWS:

PARCEL FOUR:

BEING A PORTION OF PARCEL ONE AS SAID PARCEL ONE IS SHOWN ON THAT CERTAIN RECORD OF SURVEY FILED ON JANUARY 26, 1985, IN VOLUME 43 OF MAPS, PAGE 4, RECORDS OF SANTA CRUZ COUNTY, AND ALSO BEING A PORTION OF THE LANDS DESCRIBED IN SAID LAST MENTIONED LANDS...

PARCEL FIVE:

BEING A PORTION OF THE NORTHEASTERN WALL LINE OF VAN NESS AVENUE FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 112 FEET SOUTHWESTERLY FROM THE MOST SOUTHERN CORNER OF LAND NOW OWNED BY BAKER AND COMPANY...

PARCEL SIX:

BEING A PORTION OF THE NORTHEAST SIDE OF ROODRIGUEZ STREET AT A POINT DISTANT SOUTH 41° EAST 250.00 FEET FROM THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL SEVEN:

BEING A PORTION OF THE SOUTHWESTERN WALL LINE OF MAIN STREET AT A POINT FROM WHICH THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE BEARS NORTH 41° WEST 250.00 FEET DISTANT AND A BEARS TABLET SET IN THE GROUND ON THE SOUTHWESTERN SIDE...

PARCEL EIGHT:

BEING A PORTION OF THE SOUTHWESTERN CORNER OF THE BUILDING KNOWN AS THE "POD" BLDG. AT THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE AND MAIN STREET 185 FEET WESTERLY THERE WESTERLY AT RIGHT ANGLES TO SAID MAIN STREET...

PARCEL NINE:

COMMENCING AT A POINT ON THE NORTHEASTERN WALL LINE OF MAIN STREET FORMERLY THIRD STREET AT A POINT FROM WHICH THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE BEARS NORTH 41° WEST 250.00 FEET DISTANT...

PARCEL TEN:

BEING A PORTION OF THE EASTERN MOST CORNER OF THE LOT OF LAND CONVEYED BY CHARLES FORD COMPANY TO VERA WALKER, DATED SEPTEMBER 29, 1970, IN VOLUME 219 OF DEEDS, PAGE 428, OFFICIAL RECORDS OF SANTA CRUZ COUNTY...

PARCEL ELEVEN:

COMMENCING AT A POINT ON THE SOUTHWESTERN WALL LINE OF LAND FORMERLY OF ONE GUMWOOD NOW OF CHARLES FORD COMPANY, AND DISTANT THEREON NORTH 41° EAST 40.00 FEET FROM THE NORTHEASTERN WALL LINE OF VAN NESS AVENUE...

PARCEL TWELVE:

COMMENCING AT A POINT ON THE SOUTHWESTERN WALL LINE OF LAND FORMERLY OF ONE GUMWOOD NOW OF CHARLES FORD COMPANY, AND DISTANT THEREON NORTH 41° EAST 40.00 FEET FROM THE NORTHEASTERN WALL LINE OF VAN NESS AVENUE...

PARCEL THIRTEEN:

COMMENCING AT THE EASTERN MOST CORNER OF THE LAND CONVEYED BY F. A. KILBURN AND FLORENCE L. KILBURN TO BAKER AND COMPANY BY DEED DATED SEPTEMBER 29, 1970, IN VOLUME 219 OF DEEDS, PAGE 428, OFFICIAL RECORDS OF SANTA CRUZ COUNTY...

PARCEL FOURTEEN:

BEING A PORTION OF THE EASTERN MOST CORNER OF A LOT OF LAND CONVEYED BY CHARLES FORD COMPANY TO VERA WALKER, DATED SEPTEMBER 29, 1970, IN VOLUME 219 OF DEEDS, PAGE 428, OFFICIAL RECORDS OF SANTA CRUZ COUNTY...

PARCEL FIFTEEN:

BEING A PORTION OF THE NORTHEASTERN SIDE OF VAN NESS AVENUE FORMERLY ROODRIGUEZ STREET, DISTANT THEREON NORTH 41° WEST 100 FEET FROM THE NORTHEASTERN SIDE OF MAIN STREET...

PARCEL SIXTEEN:

COMMENCING AT THE INTERSECTION OF THE NORTHEASTERN SIDE OF ROODRIGUEZ STREET ON VAN NESS AVENUE, WITH THE SOUTHWESTERN WALL LINE OF LAND CONVEYED TO M.V. MOFFIT BY DEED RECORDED IN VOLUME 122 OF DEEDS, PAGE 103, OFFICIAL RECORDS OF SANTA CRUZ COUNTY...

PARCEL SEVENTEEN:

COMMENCING AT THE INTERSECTION OF THE NORTHEASTERN SIDE OF ROODRIGUEZ STREET ON VAN NESS AVENUE, WITH THE SOUTHWESTERN WALL LINE OF LAND CONVEYED TO M.V. MOFFIT BY DEED RECORDED IN VOLUME 122 OF DEEDS, PAGE 103, OFFICIAL RECORDS OF SANTA CRUZ COUNTY...

PARCEL EIGHTEEN:

COMMENCING ON THE NORTH EAST SIDE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, AND RUNNING THEREON SOUTH 41° WEST 100 FEET TO THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL NINETEEN:

BEING A PORTION OF THE NORTH EAST LINE OF VAN NESS AVENUE AT A STATION FROM WHICH THE NORTH EAST LINE OF VAN NESS AVENUE BEARS NORTH 41° WEST 250.00 FEET DISTANT...

PARCEL TWENTY:

COMMENCING AT THE POINT OF INTERSECTION OF THE NORTHWESTERN WALL LINE OF THIRD STREET WITH THE NORTHWESTERN WALL LINE OF ROODRIGUEZ STREET, WHICH POINT IS LOCATED AT THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY ONE:

BEING A PORTION OF PARCEL ONE OF THE LANDS CONVEYED TO CHARLES FORD COMPANY, A CALIFORNIA CORPORATION, BY DEED RECORDED IN VOLUME 128, PAGE 230, OFFICIAL RECORDS OF SANTA CRUZ COUNTY...

PARCEL TWENTY TWO:

COMMENCING AT A POINT IN THE SOUTHWEST WALL LINE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 125 FEET SOUTH OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY THREE:

COMMENCING AT A POINT IN THE SOUTHWEST WALL LINE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 125 FEET SOUTH OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY FOUR:

COMMENCING AT A POINT IN THE SOUTHWEST WALL LINE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 125 FEET SOUTH OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY FIVE:

COMMENCING AT A POINT IN THE SOUTHWEST WALL LINE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 125 FEET SOUTH OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY SIX:

COMMENCING AT A POINT IN THE SOUTHWEST WALL LINE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 125 FEET SOUTH OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY SEVEN:

COMMENCING AT A POINT IN THE SOUTHWEST WALL LINE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 125 FEET SOUTH OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY EIGHT:

COMMENCING AT A POINT IN THE SOUTHWEST WALL LINE OF VAN NESS AVENUE, FORMERLY ROODRIGUEZ STREET, SAID POINT BEING 125 FEET SOUTH OF THE INTERSECTION OF THE SOUTHWESTERN WALL LINE OF VAN NESS AVENUE WITH THE SOUTHWESTERN WALL LINE OF MAIN STREET...

PARCEL TWENTY NINE:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY ONE:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY TWO:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY THREE:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY FOUR:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY FIVE:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY SIX:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY SEVEN:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL THIRTY EIGHT:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

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PARCEL FORTY:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

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PARCEL FORTY THREE:

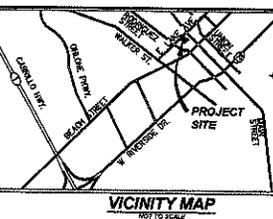
BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL FORTY FOUR:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

PARCEL FORTY FIVE:

BEING A PORTION OF PARCEL NUMBER 1 AND A PORTION OF PARCEL NUMBER 4 OF THE LANDS DESCRIBED IN THE MATTER OF THE ESTATE OF ROBERTSON K. KROHN, DECEASED, IN THE OFFICE SETTLE LINE FIRST AND FINAL ACCOUNT AND OF FINAL DISTRIBUTION RECORDED IN VOLUME 309 OF DEEDS, PAGE 303, SANTA CRUZ COUNTY RECORDS AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:



LEGAL DESCRIPTION FOR
AutoZone STORE NO. 6260
WEST LAKE AVENUE
WATSONVILLE, CA 95076
APN: 017-641-09

EXCEPTIONS:

- ITEM 1: GENERAL AND SPECIAL TAXES AND ASSESSMENTS FOR THE FISCAL YEAR 2014-2015.
- ITEM 2: GENERAL AND SPECIAL TAXES AND ASSESSMENTS FOR THE FISCAL YEAR 2015-2016.
- ITEM 3: THE USE OF SUPPLEMENTAL TAXES, IF ANY, ASSESSED PURSUANT TO AN ORDER OF THE BOARD OF SUPERVISORS WITH SECTION 15 OF THE CALIFORNIA REVENUE AND TAXATION CODE.
- ITEM 4: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED JUNE 17, 1970 IN BOOK 222 OF DEEDS, PAGE 233, IN FAVOR OF GEORGE HAMILTON BRIMMINGTON, PLOTTED AND SHOWN HEREON.
- ITEM 5: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 6: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 7: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 8: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 9: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 10: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 11: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 12: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 13: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 14: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 15: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 16: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 17: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 18: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 19: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.
- ITEM 20: AN EASEMENT FOR PUBLIC UTILITIES AND INCIDENTAL PURPOSES, RECORDED FEBRUARY 18, 1973 IN BOOK 245 OF DEEDS, PAGE 188, IN FAVOR OF CALIFORNIA GAS AND ELECTRIC COMPANY, PLOTTED AND SHOWN HEREON.

NOTES:

- 1. THE TITLE REPORT USED FOR THIS SURVEY WAS ISSUED BY FIRST AMERICAN TITLE COMPANY NATIONAL COMMERCIAL SERVICES AND IS NUMBERED NCS-8291-30 AND DATED JANUARY 14, 2015.
- 2. THE PERMITS FOR CONSTRUCTION FOR THIS PARCEL IS THE SOURCE OF THE VERTICAL AND HORIZONTAL TOPOGRAPHY SHOWN HEREON ON A FIELD GROUND SURVEY.
- 3. THE TOPOGRAPHIC CLASSIFICATION FOR THIS PARCEL IS C-2.
- 4. THERE IS NO EVIDENCE OF CURRENT GRADING OR BUILDING CONSTRUCTION ON THIS PARCEL.
- 5. THERE IS CURRENTLY NO INCINERATOR RIGHT OF WAY CHANGES WITHIN THE TOWN OF WATSONVILLE WITHIN THE BOUNDARIES OF THE WATSONVILLE 2000 REGULATORY PROJECT AREA AS DISCLOSED BY THE COUNTY PUBLIC UTILITIES AND INCIDENTAL PURPOSES DEPARTMENT ON FEBRUARY 11, 2015 IN BOOK 245 OF DEEDS, PAGE 188.
- 6. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 7. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 8. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 9. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 10. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 11. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 12. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 13. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 14. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 15. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 16. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 17. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 18. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 19. THERE ARE NO DELINEATED METHODS ON THIS SITE.
- 20. THERE ARE NO DELINEATED METHODS ON THIS SITE.

Table with columns: DATE, APPROVED, REVISION, CHECK, DATE, DESIGN, DRAWN, QUANTITY, SCALE, and other project details.

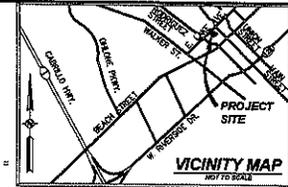
PAVING LEGEND:

HEAVY DUTY AC	
HEAVY DUTY PCC	
LIGHT DUTY PCC	
PEDESTRIAN PCC	
LANDSCAPING	

SHEET INDEX:

- C1 - PRELIMINARY SITE PLAN
- C2 - PRELIMINARY GRADING PLAN
- C3 - PRELIMINARY UTILITY PLAN
- C4 - PRELIMINARY STORM WATER CONTROL PLAN

PRELIMINARY SITE PLAN FOR
AutoZone STORE NO. 6260
 WEST LAKE AVENUE
 WATSONVILLE, CA 95076
 APN: 017-641-09 INSTRUMENT NO. 2009-003296



BASIS OF BEARINGS:

THE BASIS OF BEARINGS FOR THIS SURVEY IS BASED ON N 1/4 18 AND WAS ESTABLISHED BY THE BEST FIT OF IMPROVEMENTS. ALL MONUMENTS PER SURVEY MAPS IN THIS BLOCK HAVE BEEN OBLITERATED BY NEW IMPROVEMENTS.

BENCHMARK:

THE BENCHMARK FOR THIS ALTA IS A 2" BRASS DISC STAMPED CM4100 SET IN THE NORTHEAST END OF THE CURB RETURN OF THE NORTHERLY CURB FOR THE INTERSECTION OF WEST LAKE AVENUE AND RODRIGUEZ STREET AND IS 2' SOUTHEAST OF A SIGNAL LIGHT POLE. THIS BENCHMARK IS 64.119' AND DESIGNATED W.B. ELEVATION = 39.90 FEET (NAVD 88).

ZONING:

THE ZONING CLASSIFICATION FOR THIS PARCEL IS: CCA-CENTRAL COMMERCIAL CORE AREA.

FLOOD ZONE:

ZONE X PER FEMA FLOOD MAP 0607-C-0392-E DATED 5/16/12

DEVELOPER:

AUTOZONE, INC.
 122 S. HAVERT STREET
 MEMPHIS TN 38103
 PH: (901) 465-8714

UTILITY COMPANIES:

UTILITIES SHOWN ARE BASED ON LIMITED DATA. THERE IS NO GUARANTEE AS TO THEIR LOCATION OR SIZE. PLEASE CONTACT RESPECTIVE UTILITY COMPANIES PRIOR TO CONSTRUCTION. POSSIBLE CONTACTS IN THIS AREA ARE:

WATER, SEWER AND REUSE/RECYCLING:
 CITY OF WATSONVILLE PUBLIC WORKS AND UTILITIES
 260 MAIN STREET, WATSONVILLE, CA 95070
 PH: 831-768-3433

IRIG: 320 MAIN STREET (831)768-3000

ELECTRIC: PO&E 515 7TH AVE SANTA CRUZ, CA 95062 (800) 743-8200

CABLE: PO&E 515 7TH AVE SANTA CRUZ, CA 95062 (800) 743-8200

CABLE-CHARTER COMMUNICATIONS 8150 CAMINO AVORADO GILROY, CA 95020 (408) 335-0751

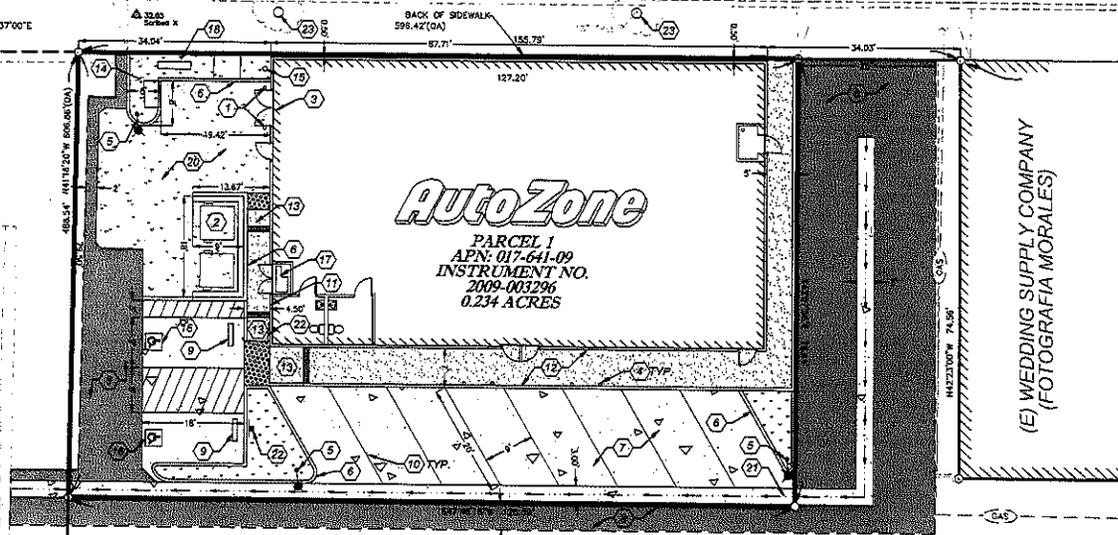
(E) CVS PHARMACY

NOTES:

- 1. PROPOSED BUILDING IS TO BE FIRE SPRINKLERED.

CL 10" PO&E & PLAT EASEMENT PER 2278 OR 415. ITEM 14 OF TITLE REPORT ALSO FOR 2290 OR 516. ITEM 17 OF TITLE REPORT.

WEST LAKE AVE.



KEYNOTES:

- BUILDING & PAVING**
1. FIRE GUARD.
 2. DUMPSTER LA YOUT - PER CITY STANDARD PLAN.
 3. SERVICE DOOR PLAN.
 4. SOLLARD PLAN.
 5. CONCRETE LIGHT POLE BASE - AIM LIGHT FIXTURE IN DIRECTION AS INDICATED.
 6. 8" VERTICAL CURB.
 7. CONCRETE PAVING, EXPANSION AND CONTROL JOINTS, MAXIMUM SPACING FOR CONTROL JOINTS IS 15' O.C. EACH WAY.
 8. ASPHALT PAVING.
 9. 3" X 6" LONG CONCRETE WHEEL STOP MOUNTED TO PAVEMENT.
 10. 4" WIDE PARKING STRIPE PAINTED WHITE (TYP).
 11. ACCESSIBLE PARKING SIGN, S.C. TO PROVIDE ONE VAN ACCESSIBLE SIGN MOUNTED TO WALL.
 12. MONOLITHIC CONCRETE CURBS AND SIDEWALK.
 13. ACCESSIBLE RAMP.
 14. NEW LANDSCAPE AREA.
 15. FREEZELESS YARD HYDRANT.
 16. PAINT INTERNATIONAL SYMBOL OF ACCESSIBILITY.
 17. MAIN ELECTRICAL SWITCH AND GAS LOCATION. SEE ARCHITECTURAL AND ELECTRICAL PLANS FOR DETAILS.
 18. AUTOZONE SIGN - SEE ARCH PLANS FOR DETAILS.
 19. ACCESSIBLE ENTRANCE SIGN.
 20. LOADING AREA: SLOPE AT MAX. 2% AWAY FROM BUILDING.
 21. 3" CONCRETE VALLEY GUTTER.
 22. ACCESSIBLE PARKING SIGN MOUNTED ON POLE.
 23. INSTALL CITY TREE GRATE. SEE LANDSCAPE PLANS.
- GENERAL NOTES:**
1. PROOF ROLL BUILDING AND ALL PARKING AREAS. NOTIFY THE ARCHITECT OF ANY UNACCEPTABLE AREAS.
 2. EDGE OF NEW PAVEMENT TO BE FLUSH WITH EXISTING PAVEMENT.
 3. ALL SIDEWALK, CURB AND GUTTER, STREET PAVING, CURB CUTS, DRIVEWAY APPROACHES, HANDICAP RAMP, ETC. CONSTRUCTION OUTSIDE THE PROPERTY LINE IN THE RIGHT-OF-WAY SHALL CONFORM TO ALL MUNICIPAL AND STATE SPECIFICATIONS AND REQUIREMENTS.
 4. FOR AREAS OUTSIDE THE PROPERTY LINE, REPAIR AND/OR REPLACE ALL DAMAGE DONE TO EXISTING UTILITIES, SIDEWALKS, PAVING, LANDSCAPING, ETC.) AS REQUIRED BY OWNER AND/OR GOVERNING AUTHORITY.
 5. FOR PROPOSED UTILITY LOCATIONS, SEE THE UTILITY PLAN.
 6. DIMENSIONS ARE TO THE FACE OF CURB UNLESS SPECIFIED OTHERWISE.

PARKING INFORMATION

ITEM	REQUIREMENTS	PROVIDED
MIN. DRIVEWAY WIDTH	16 FT	16 FT
MIN. DRIVEWAY WIDTH	18 FT	18 FT
HANDICAP SPACES	1 SPACES	1 SPACES
LOADING SPACE		

BULK AREA REQUIREMENTS

ITEM	REQUIREMENTS	PROVIDED
MINIMUM LOT FRONTAGE	6 FT	6 FT
MINIMUM FRONT SETBACK	0 FT	0 FT
MINIMUM SIDE SETBACK	0 FT	0 FT
MINIMUM REAR SETBACK	0 FT	0 FT

APPROXIMATE LOCATION OF CL PIPELINE EASEMENT (NO WIDTH) TO COAST COUNTRIES GAS & ELECTRIC CO. PER 281 OR 58, ITEM 9 OF TITLE REPORT.



DATE: 07-10-2014
 REVISION: 1
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 PROJECT NO: 017-641-09
 SHEET NO: C1
 OF TOTAL: 4
 DATE: 07-10-2014

AutoZone, Inc.
 122 S. HAVERT STREET
 MEMPHIS, TN 38103
 CONTACT: AUTOZONE, INC.
 PH: (901) 465-8714

ENGINEERING, INC.
 1000 W. 10TH STREET
 WATSONVILLE, CA 95076
 PH: (831) 768-3433

017-641-09 STORE NO. 6260
 WEST LAKE AVENUE
 WATSONVILLE, CA 95076
PRELIMINARY SITE PLAN
 REF. PROJECT #1030 - AUTOZONE, WEST LAKE AVENUE, WATSONVILLE, CA 95076

PAVING LEGEND:

HEAVY DUTY AC	
HEAVY DUTY PCC	
LIGHT DUTY PCC	
PEDESTRIAN PCC	
LANDSCAPING	

BASIS OF BEARINGS:

THE BASIS OF BEARINGS FOR THIS SURVEY IS BASED ON 9 PM 18 AND WAS ESTABLISHED BY THE BEST FIT OF IMPROVEMENTS. ALL MONUMENTS PER SURVEY MAPS IN THIS BLOCK HAVE BEEN OBLITERATED BY NEW IMPROVEMENTS.

BENCHMARK:

THE BENCHMARK FOR THIS ALTA IS A 2" BRASS DISC STAMPED CARLOS SET IN THE NORTH EAST END OF THE CURB RETURN OF THE NORTHERLY CURB FOR THE INTERSECTION OF WEST LAKE AVENUE AND RODRIGUEZ STREET AND IS 2' SOUTHEAST OF A SIGNAL LIGHT POLE. NOS. P10 IS QUARIP AND DESIGNATED W/ ELEVATION = 39.90 FEET (NAVD 83).

ZONING:

THE ZONING CLASSIFICATION FOR THIS PARCEL IS: CCL-CENTRAL COMMERCIAL CORE AREA.

FLOOD ZONE:

ZONE X PER FEMA FLOOD MAP 06071-C-0315-E DATED 6/16/12

DEVELOPER:

AUTOZONE, INC.
125 S. FRONT STREET
MEMPHIS TN 38013
PH: (901) 455-8714

UTILITY COMPANIES:

UTILITIES SHOWN ARE BASED ON LIMITED DATA. THERE IS NO GUARANTEE AS TO THEIR LOCATION OR SIZE. PLEASE CONTACT RESPECTIVE UTILITY COMPANIES PRIOR TO CONSTRUCTION. POSSIBLE CONTACTS IN THIS AREA ARE:

WATER, SEWER AND REUSE/RECYCLING:
CITY OF WATSONVILLE PUBLIC WORKS AND UTILITIES
256 MAIN STREET, WATSONVILLE, CA 95076
PH: 831.745.5400

FIRE:

256 MAIN STREET
WATSONVILLE, CA 95076
PH: 831.745.5400

ELECTRIC: PG&E

615 7TH AVE
SANTA CRUZ, CA 95062
PH: 831.745.3000

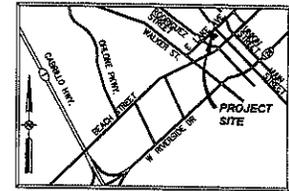
GAS: PG&E

615 7TH AVE
SANTA CRUZ, CA 95062
PH: 831.745.3000

NOTES:

1. PROPOSED BUILDING IS TO BE FIRE SPRINKLERED.

PRELIMINARY GRADING PLAN FOR
AutoZone STORE NO. 6260
WEST LAKE AVENUE
WATSONVILLE, CA 95076
APN: 017-641-09 INSTRUMENT NO. 2009-003296

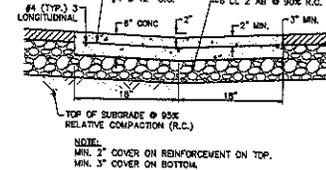


VICINITY MAP
NOT TO SCALE

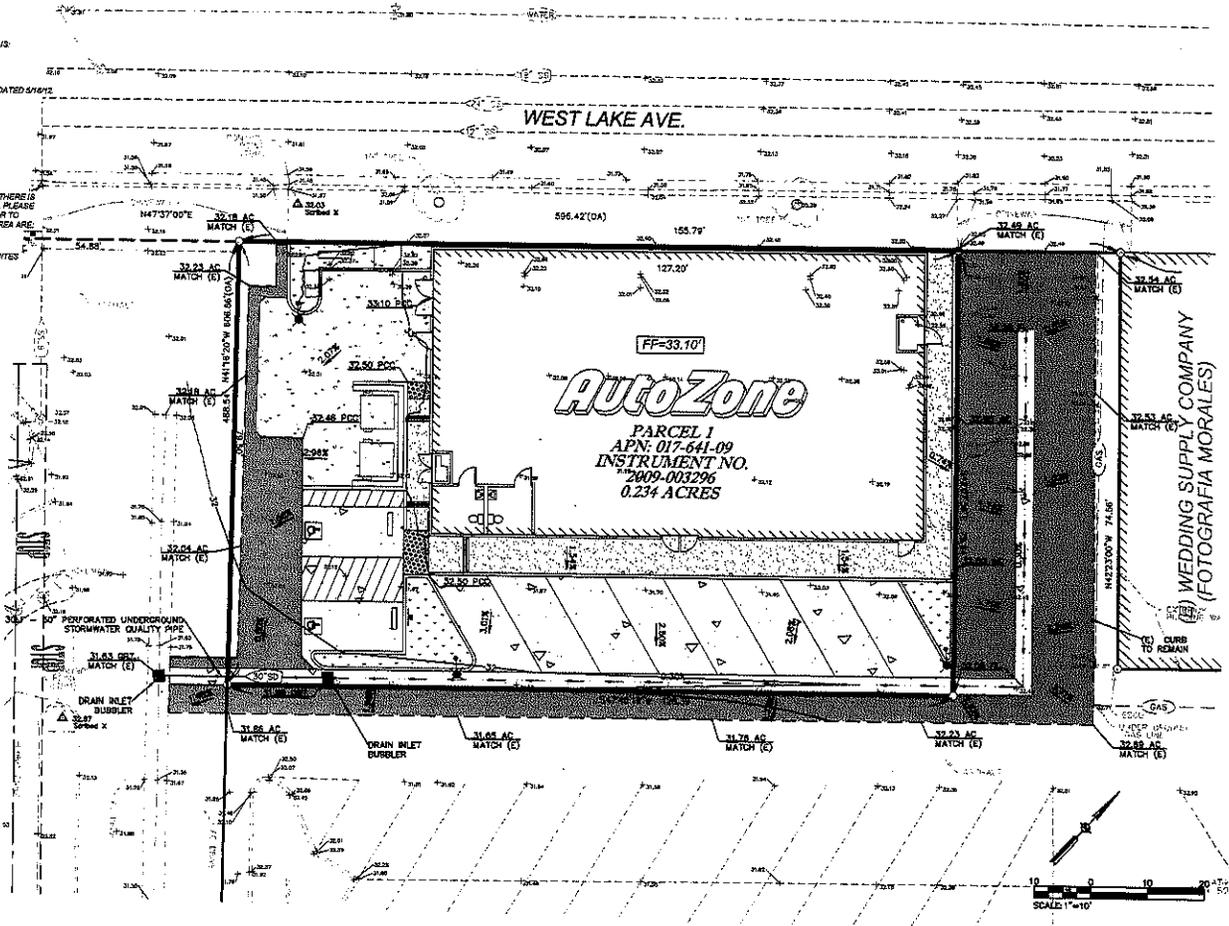
LEGEND:

DESCRIPTION	EXISTING	PROPOSED
PROPERTY LINE		
RIGHT-OF-WAY		
BASEMENT		
LOT LINE		
CENTERLINE		
SW CURB & GUTTER		

CONTROL POINT		
ROLLAR		
UTILITY POLE		
STREET LIGHT		
INDEX CONTOUR		
INTERMEDIATE CONTOURS		
SIGN		
TREE & DRIP		
HANDICAP RAMP		
TRUNCATED DOWNS		
BUILDING		
TEMPORARY SITE BENCHMARK		
GRADE BREAK LINE		
FINISH FLOOR ELEVATION	FF=123.00	FF=123.00
SPOT ELEVATION	13.34 AC	13.34 AC
SPOT ELEVATION (ASPHALT CONCRETE)	13.81 AC	13.81 AC
MATCH (E) GRADE ELEVATION		



3' CONCRETE VALLEY GUTTER
NOT TO SCALE

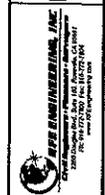


NO.	REVISION	DATE	BY	APP'D BY

NO.	DESCRIPTION	DATE

NO.	DESCRIPTION	DATE

NO.	DESCRIPTION	DATE



AUTOZONE INC.
125 S. FRONT STREET
MEMPHIS, TN 38013
CONTRACT NO. 017-641-09-001
PH: (901) 455-8714

AutoZone STORE NO. 6260
WEST LAKE AVENUE
WATSONVILLE, CA 95076
PRELIMINARY GRADING PLAN



Know what's below.
Call before you dig.
or (800) 227-2600

Drawing	Sheet

07-10-2014

PROJECT #14030 - AUTOZONE WEST LAKE AVENUE, WATSONVILLE, CA 95076

AUTOZONE STORE

WEST LAKE AVENUE, WATSONVILLE, CALIFORNIA 95076

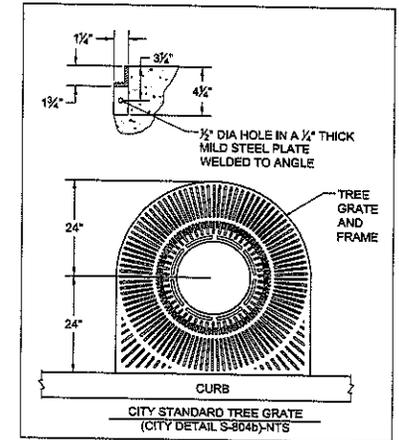
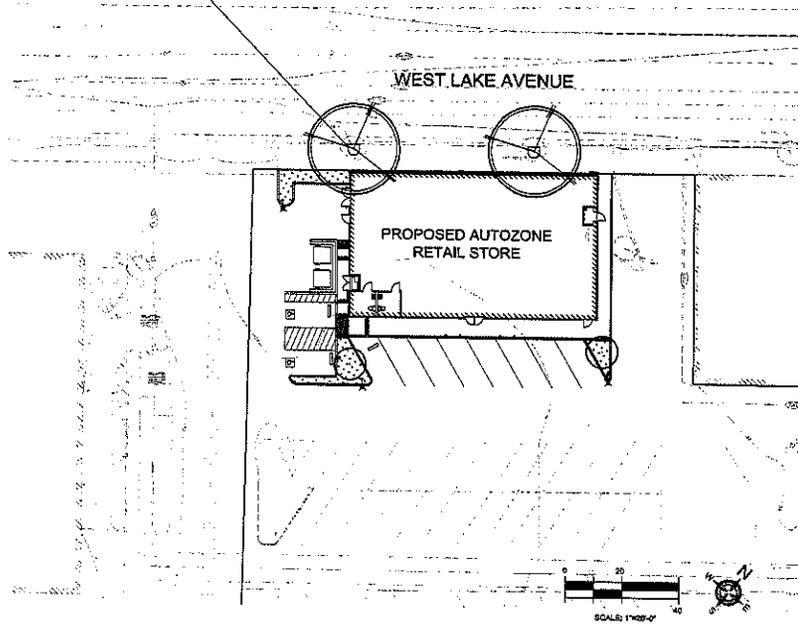
PLANTING NOTES

1. PLANTING SHALL CONFORM TO LOCAL REQUIREMENTS FOR LANDSCAPE SITE DEVELOPMENT.
2. PROVIDE MINIMUM SLOPE OF 1 % FOR POSITIVE DRAINAGE AWAY FROM CENTER IN ALL PLANTED AREAS.
3. THE PLANT QUANTITIES SHOWN ON THE DRAWINGS ARE INFORMATIONAL ONLY. THE CONTRACTOR IS RESPONSIBLE FOR FINAL QUANTITIES REQUIRED TO COMPLETE THE WORK. IN CASE OF DISCREPANCY, THE PLAN SHALL GOVERN.
4. ALL TREES SHALL BE PLANTED A MINIMUM OF 2' FROM UNDERGROUND UTILITIES.
5. ALL EXISTING TREES SHALL BE PROTECTED FROM DAMAGES OR INJURY. NO PARKING OR STACKING OF CONSTRUCTION MATERIAL IS ALLOWED WITHIN THE DRILLLINE OF AN EXISTING TREE.
6. IMMEDIATELY AFTER AWARD OF CONTRACT, THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT IF SPECIFIED PLANT MATERIAL IS AVAILABLE FROM COMMERCIAL NURSERIES. IN THE EVENT THAT A PLANT IS NOT AVAILABLE, THE LANDSCAPE ARCHITECT WILL PROVIDE ALTERNATE PLANT MATERIAL SELECTIONS. SUCH CHANGES WILL NOT ALTER THE CONTRACTOR'S ORIGINAL BID PRICE UNLESS A CREDIT IS DUE TO THE OWNER.
7. THE CONTRACTOR SHALL ENSURE THAT ALL EXCAVATED PLANT FITS HAVE POSITIVE DRAINAGE. PLANT FITS SHALL BE FULLY FILLED WITH WATER AND SHALL DRAIN WITHIN ONE (1) HOUR OF FILLING. THE CONTRACTOR SHALL EXCAVATE THROUGH ANY IMPERVIOUS LAYER IF ENCOUNTERED.
8. ALL PLANT MATERIAL SHALL COMPLY WITH AND/ OR 2001 STANDARD FOR NURSERY STOCK.
9. ROOT BARRIERS SHALL BE PROVIDED FOR ALL TREES WITHIN 5' OF ANY CURB AND/OR HARDSCAPE PAVEMENT.
10. ALL PLANTER AREAS SHALL RECEIVE A 3" LAYER OF BARK MULCH.
11. THE CONTRACTOR SHALL PROVIDE A SOILS REPORT PREPARED BY A QUALIFIED SOILS SPECIALIST AND SUBMIT TO THE OWNER FOR FINAL APPROVAL. SOILS SAMPLES SHALL BE COLLECTED AFTER ROUGH GRADING OPERATIONS AND PRIOR TO THE INSTALLATION OF PLANT MATERIAL. SOIL SAMPLES SHALL BE SUFFICIENTLY NUMEROUS TO ACCOUNT FOR ANY SOIL VARIATIONS THAT MAY BE PRESENT ON THE SITE. THE FOLLOWING MINIMUM ITEMS SHALL BE INCLUDED IN THE ANALYSIS:
 - A. INFILTRATION RATE.
 - B. SOIL TEXTURE.
 - C. CATION EXCHANGE CAPACITY.
 - D. SOIL FERTILITY INCLUDING TESTS FOR NITROGEN, POTASSIUM, PHOSPHOROUS, PH, ORGANIC MATTER AND SPECIFIC CONDUCTANCE (EC).
12. PRIOR TO PLANTING, SOIL AMENDMENTS SHALL BE ADDED PER RECOMMENDATIONS OF THE SOILS REPORT. SOIL AMENDMENTS SHOWN ON THE PLANS ARE TO BE USED FOR BIDDING PURPOSES ONLY. THE RESULTS OF THE SOILS TESTS THE CONTRACTOR PERFORMS SHALL DETERMINE ACTUAL AMENDMENTS.

PLANT LEGEND

SYM	BOTANICAL/COMMON NAME	SIZE	QTY	REMARKS
	TREES			
	JACARANDA MIMOSIFOLIA	24" BOX	2	STANDARD
	LAGERSTROEMIA INDICA MUSKOGEE/CRAPE MYRTLE	15 GAL	2	STANDARD
	SHRUBS/SPRINGHOUSE			
	DIESEL VEGETA/FORTNIGHT LILY	1 GAL	-	
	MUHLENBERGIA RUPESTRIS/DEER GRASS	1 GAL	-	
	ROSMARINUS OFFICINALIS TUSCAN BLUE	5 GAL	-	

INSTALL CITY STANDARD TREE GRATE (CITY DETAIL S-804b)



AutoZone
123 S. FRONT STREET
MEMPHIS, TN 38103
CONTACT: MITCH BRAMLITT
PH: (901) 495-8714

PRELIMINARY LANDSCAPE PLAN

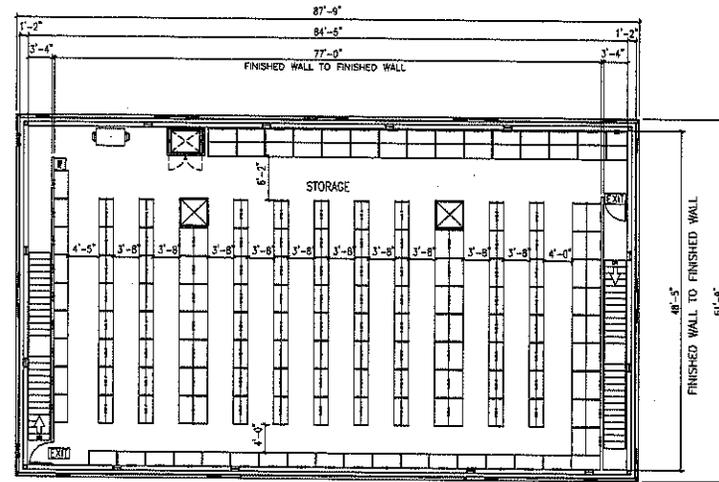
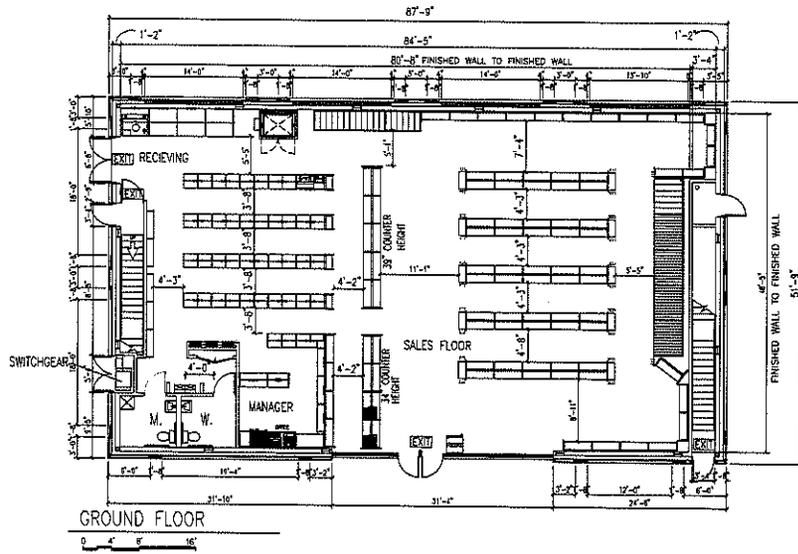
JULY 10, 2014

MELA
LANDSCAPE ARCHITECTURE
306 Canyon Falls Dr. Folsom, CA 95630
ph/fax: 916.968.3372 www.meladesign.com



SHEET
P1
OF
TOTAL
1

AUTOZONE STORE NO 6280, WEST LAKE AVENUE, WATSONVILLE, CALIFORNIA MSLA PROJECT NO. 57814



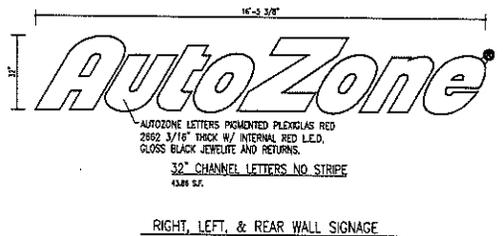
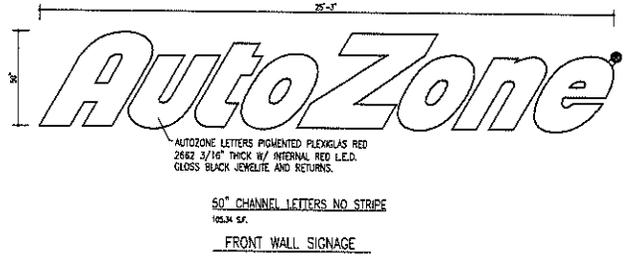
DESIGN CODES: 2013 CALIFORNIA BUILDING CODE
 OCCUPANCY CLASSIFICATION: 1st FLOOR M (MERCANTILE) 2nd FLOOR S (STORAGE)
 TYPE OF CONSTRUCTION: III B
 NUMBER OF STORIES: 2 ALLOWED / 2 SHOWN
 GROUND FLOOR AREA: 4522 S.F.
 SECOND FLOOR AREA: 4451 S.F.
 TOTAL BUILDING AREA ALLOWED 12,500 S.F.
 TOTAL BUILDING AREA SHOWN 8973 S.F.
 AREA INCREASE: NOT APPLICABLE

GROUND FLOOR USE: MERCANTILE
 MERCANTILE OCCUPANCY: 30 S.F. PER PERSON
 GROUND FLOOR OCCUPANCY: 4522 / 30 = 150
 SECOND FLOOR USE: OVERSTOCK (STORAGE)
 STORAGE OCCUPANCY: 300 S.F. PER PERSON
 SECOND FLOOR OCCUPANCY: 4451 / 300 = 15
 TOTAL OCCUPANCY FOR BUILDING: 165



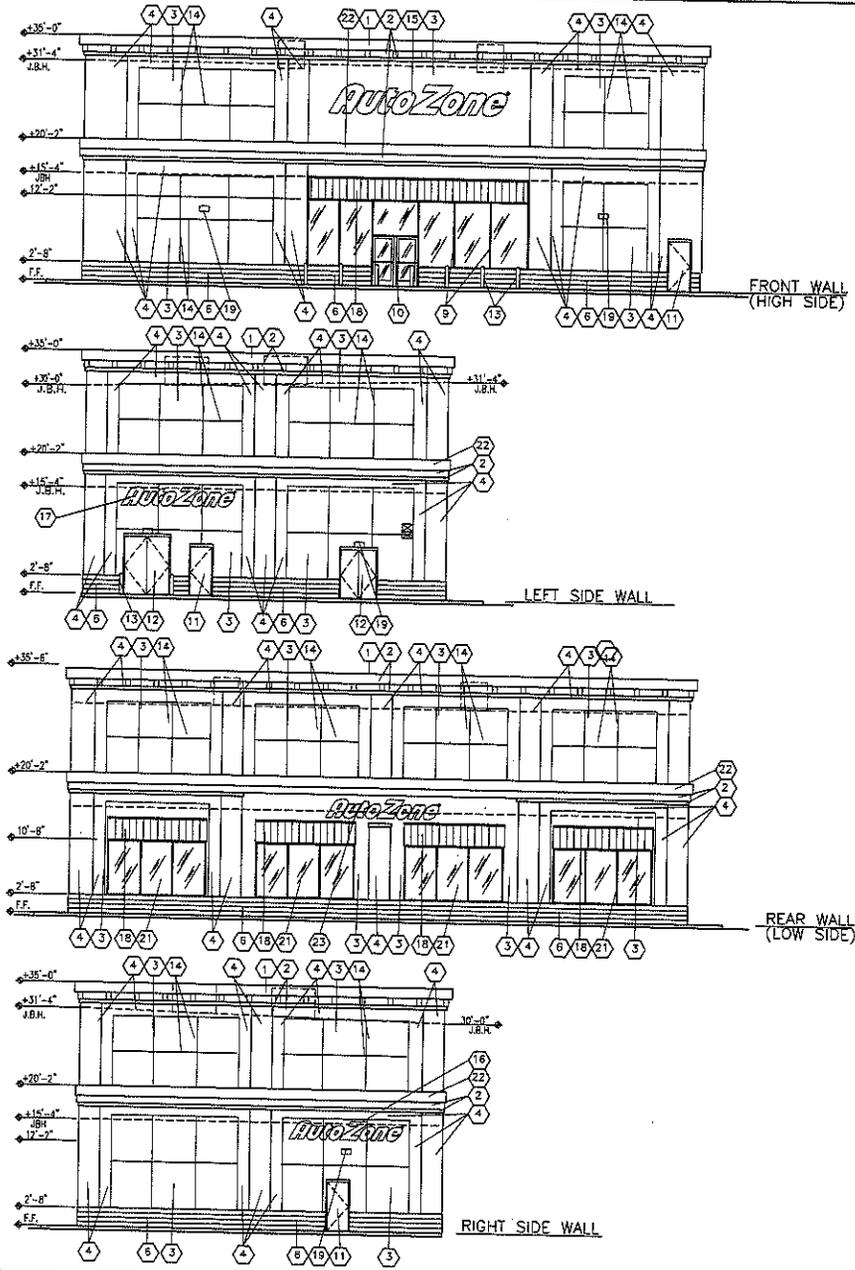
1 FIXTURE PLANS

FLOOR PLANS



2 BUILDING MOUNTED SIGNAGE

AutoZone Store No. 6260		LAKE STREET		WATSONVILLE CA 95076	
Architect:		123 South Front Street		Memphis, Tennessee 38103	
TEL: 901-495-8714		FAX: (901) 495-8969		For Bidding & Contractor Information Contact:	
McGraw - Hill Construction		Tel. 615-884-1017		www.construction.com	
REVISONS		1	2	3	4
		5	6		
FLOOR PLANS		PS1			



- 1 CORNICE - STUCCO FINISH COLOR 3
- 2 CORNICE - STUCCO FINISH COLOR 2
- 3 STUCCO FINISH - COLOR 2
- 4 STUCCO FINISH - COLOR 3
- 5 HORIZONTAL PROJECTIONS - STUCCO FINISH COLOR 3
- 6 SMOOTH FACE MASONRY VENEER UNITS, COLOR - 3
- 7 ALUMINUM STOREFRONT - RED KYMAR FINISH
- 8 STORE FRONT ALUMINUM DOORS - CLEAR ANODIZED FINISH
- 9 PAINT MARY DOOR RED & METAL FRAMES BLACK
- 10 PAINT DOUBLE DOORS RED & METAL FRAMES BLACK
- 11 PIPE GUARD WITH RED SLEEVE
- 12 1" V GROOVE SCORE JOINT
- 13 FRONT WALL SIGN - 50" AUTOZONE NO STRIPES
- 14 RIGHT SIDE WALL SIGN - 32" AUTOZONE RIGHT STRIPES
- 15 LEFT SIDE WALL SIGN - 32" AUTOZONE LEFT STRIPES
- 16 TEE-PANEL ROOFING METAL ANNING COLOR UNA-CLOA BRANDBYNE RED
- 17 WALL MOUNTED LED LIGHT FIXTURE
- 18 STORE ADDRESS - 6" WHITE REFLECTIVE NUMBERS
- 19 ALUMINUM STOREFRONT - RED KYMAR FINISH WITH BLACK OPAQUE GLAZING
- 20 HORIZONTAL PROJECTIONS - STUCCO FINISH COLOR A2 RED
- 21 REAR SIDE WALL SIGN - 32" AUTOZONE RIGHT STRIPES

1 ELEVATION KEYNOTES

- 1 - SHERWIN WILLIAMS #6083 SABLE
- 2 - SHERWIN WILLIAMS #6086 SAND DUNE
- 3 - SHERWIN WILLIAMS #6088 NUTMATCH

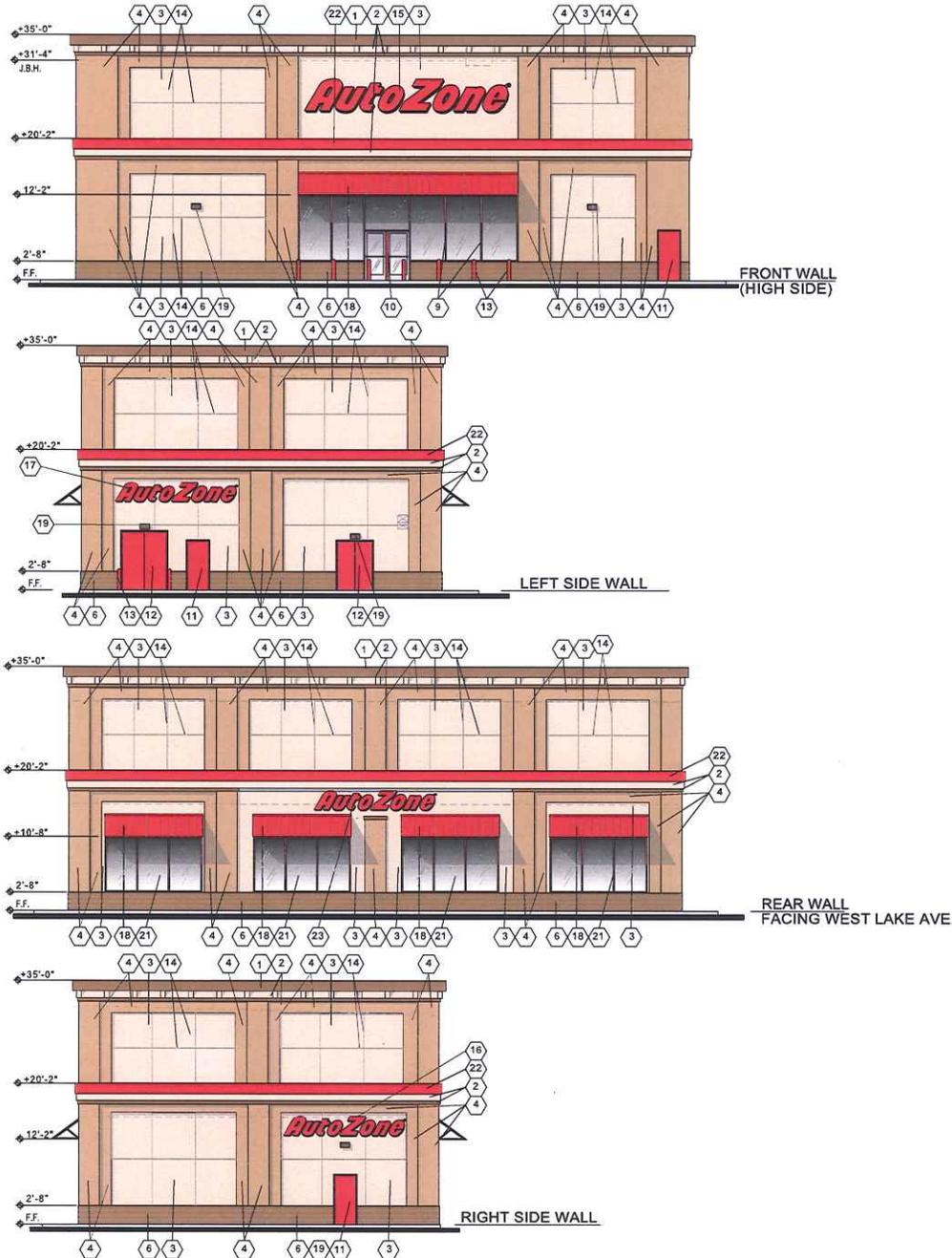
2 COLOR SCHEDULE

REVISIONS			
1			
2			
3			
4			
5			
6			

AutoZone Store No. 6260
 LAKE STREET
 WATSONVILLE CA 95076
BUILDING ELEVATIONS

Architect:
 173 South Front Street
 Memphis, Tennessee 38103
 TEL: 901-495-8714, FAX: (901) 495-8969
 For Bidding & Contractor Information Contact:
 McGraw-Hill Construction Tel. 615-884-1017
 www.construction.com

06-26-14
 40w - L



- 1 CORNICE - STUCCO FINISH: COLOR 3
- 2 CORNICE - STUCCO FINISH: COLOR 2
- 3 STUCCO FINISH - COLOR 2
- 4 STUCCO FINISH - COLOR 3
- 5 HORIZONTAL PROJECTIONS - STUCCO FINISH: COLOR 3
- 6 SMOOTH FACE MASONRY VENEER UNITS. COLOR - 3
- 9 ALUMINUM STOREFRONT - RED KYNAR FINISH
- 10 STORE FRONT ALUMINUM DOORS - CLEAR ANODIZED FINISH
- 11 PAINT MAN DOOR RED & METAL FRAMES BLACK
- 12 PAINT DOUBLE DOORS RED & METAL FRAMES BLACK
- 13 PIPE GUARD WITH RED SLEEVE
- 14 1" V GROOVE SCORE JOINT
- 15 FRONT WALL SIGN - 50" AUTOZONE CHANNEL LETTERS
- 16 RIGHT SIDE WALL SIGN - 32" AUTOZONE CHANNEL LETTERS
- 17 LEFT SIDE WALL SIGN - 32" AUTOZONE CHANNEL LETTERS
- 18 TEE-PANEL ROOFING METAL AWNING COLOR UNA-CLAD BRANDYWINE RED
- 19 WALL MOUNTED LED LIGHT FIXTURE
- 20 STORE ADDRESS - 5" WHITE REFLECTIVE NUMBERS
- 21 ALUMINUM STOREFRONT - RED KYNAR FINISH WITH BLACK OPAQUE GLAZING
- 22 HORIZONTAL PROJECTIONS - STUCCO FINISH: COLOR AZ RED
- 23 REAR SIDE WALL SIGN - 32" AUTOZONE CHANNEL LETTERS

1 ELEVATION KEYNOTES

- 1 - SHERWIN WILLIAMS #6053 SABLE
- 2 - SHERWIN WILLIAMS #6066 SAND DUNE
- 3 - SHERWIN WILLIAMS #6088 NUTTHATCH

2 COLOR SCHEDULE

REVISIONS	
1	11/01/13
2	
3	
4	
5	
6	

AutoZone Store No. 6260
LAKE STREET
WATSONVILLE CA 95076
BUILDING ELEVATIONS

Architect: AutoZone Development
123 South Front Street
Memphis, Tennessee 38103
TEL: (901) 495-8969
FAX: (901) 495-8969

06-05-14

RESOLUTION NO. ____-14 (PC)

**RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF WATSONVILLE,
CALIFORNIA, APPROVING MINOR LAND DIVISION, SPECIAL USE AND DESIGN REVIEW
PERMIT WITH ENVIRONMENTAL REVIEW (PP2014-165) TO CREATE A 10,193 SQ. FT. LOT TO
ALLOW CONSTRUCTION OF A TWO STORY 8,973 SQ. FT. AUTOZONE RETAIL BUILDING IN A
CCA ZONING DISTRICT.**

Project: 17 West Lake Avenue. (APN: 017-641-09)

WHEREAS, an application for a Minor Land Division, Special Use and Design Permit with Environmental Review (PP2014-165) to create a 10,193 sq. ft. lot to allow the construction of a two story 8,973 sq. ft. AutoZone retail store at 17 West Lake Avenue, Watsonville, California, was filed by Mitch Bramlitt, AutoZone Corporation, applicant, on behalf of the owner, William Hansen on July 14, 2014; and

WHEREAS, the total lot area will be 10,193 sq. ft. and all site characteristics will be consistent with the provisions of the Subdivision Ordinance of the City of Watsonville (Chapter 13 of the Watsonville Municipal Code); and

WHEREAS, the Planning Commission has also considered Special Use Permit and Design Review application for the creation of a 8,973 sq. ft. two story building on the site for an auto parts retail use; and

WHEREAS, notice of time and place of the hearing to consider approval of the Minor Land Division, Special Use and Design Review Permit with Environmental Review (PP2014-165) was given at the time and in the manner prescribed by the Zoning Ordinance of the City of Watsonville. The matter called for hearing evidence both oral and documentary introduced and received, and the matter submitted for decision; and

WHEREAS, the Planning Commission has considered all written and verbal evidence regarding this application at the public hearing and has made Findings, attached hereto and marked

as Exhibit "A" for the Minor Land Division, and Conditions, attached hereto and marked as Exhibit "B" for the Minor Land Division, creating a 10,193 sq. ft. lot and further considered Findings attached hereto and marked as Exhibits "C" & "D" in support of Special Use and Design Review Permit with Environmental Review (PP2014-165) and Conditions, attached hereto and marked as Exhibit "E" to allow construction of a two story 8,973 sq. ft. retail building for an AutoZone auto parts retail store at 17 West Lake Avenue, Watsonville, California.

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Watsonville, California, as Follows:

Good cause appearing, therefore, the Planning Commission of the City of Watsonville does hereby grant approval of the Minor Land Division, Special Use and Design Review Permit with Environmental Review (PP2014-165), attached hereto and marked as Exhibit "F", subject to the Conditions attached hereto and marked as Exhibit "B", for the 10,193 sq. ft. lot created by the Minor Land Division and Exhibit "E" for the Special Use Permit and Design Review for the 8,793 sq. ft. building at 17 West Lake Avenue, Watsonville, California.

I HEREBY CERTIFY that the foregoing Resolution was introduced at a regular meeting of the Planning Commission of the City of Watsonville, California, held on the 2nd day of September, 2014, by Commissioner _____, who moved its adoption, which motion being duly seconded by Commissioner _____, was upon roll call, carried and the resolution adopted by the following vote:

Ayes: Commissioners:
Noes: Commissioners:
Absent: Commissioners:

Marcela Tavantzis, Secretary
Planning Commission

Marty Corley, Chairperson
Planning Commission

**CITY OF WATSONVILLE
MINOR LAND DIVISION**

EXHIBIT A

Minor Land Division No.: PP2014-165
APN'S: 017-641-09
Location: 17 West Lake Avenue
Applicant: Mitch Bramlitt, AutoZone Corp.
Hearing Date: September 2, 2014

REQUIRED ORDINANCE FINDINGS FOR APPROVAL (SECTION 13-8.108(d))

SUPPORTIVE EVIDENCE

<p>That the proposed subdivision, together with the provisions for its design & improvement, is consistent with the Watsonville General Plan and any specific plans.</p>	<p>The project site is designated Central Commercial Core. Approval for this Minor Land Division will allow the development of up to an 8,973 sq. ft. Auto supply store. The project meets WMC requirements per section 14.16-1003. Thus, the subdivision is consistent with the general plan.</p>
<p>The proposal is consistent with the Zoning Ordinance and any other master plan or precise development plan adopted pursuant thereto.</p>	<p>The project site is zoned CCA, which requires minimum lot sizes of 2,500 sq. ft. for interior lots. The proposed parcel is 10,193 sq. ft. with 127 feet of frontage. Thus, the proposal is consistent with the Zoning Ordinance.</p>
<p>The design of the project provides, to the extent feasible, for future passive or natural heating and cooling opportunities in the subdivision.</p>	<p>The design allows for natural heating and cooling opportunities through landscaping and future commercial building configuration.</p>
<p><u>OTHER FINDINGS:</u> That the site is physically suitable for the type of development.</p>	<p>The site will be physically suitable for development consistent with the CCA zoning district.</p>
<p>That the site is physically suitable for the proposed density of the development.</p>	<p>The soil report for the project indicates that the site is physically suitable for development.</p>
<p>That the design of the proposed improvements is not likely to cause substantial environmental damage, or substantially & unavoidably injure fish or wildlife or their habitat.</p>	<p>The development is located in an area with existing urban development, and the proposed project has been evaluated by the City to reduce potential environmental impacts. Additionally, conditions are attached to the Minor Land Division Permit to promote lower environmental impacts. Thus, there does not appear to be any potential issues that may cause environmental damage.</p>
<p>That the design of the type of improvements is not likely to cause serious public health problems.</p>	<p>The proposed subdivision will not create serious health problems, as it is has been designed in accordance with City's public improvement standards to reduce any potential health problems/concerns.</p>
<p>That the design of the type of improvements will not conflict with easements acquired by the public at large for access through, or use of</p>	<p>The proposed subdivision includes public access to the subdivision. Thus, the subdivision design will not conflict with easements acquired by the public.</p>

property within the proposed subdivision.	
That the improvement will not create a need for public services that the City is not capable of providing.	The City is currently able to provide all public services for the proposed parcels.
It is not anticipated that the discharge of waste from the existing parcels will result in violation of existing requirements prescribed by the California Regional Water Quality Control Board (CRWQCB).	The City of Watsonville's Public Works and Engineering Department has reviewed the proposed subdivision, and has conditioned the Minor Land Division Permit with appropriate discharge of waste standards. Thus, the proposed subdivision will not result in any violations of the CRWQCB.
That the proposed project meets all requirements or conditions of the Subdivision Ordinance and the State Subdivision Map Act.	The subdivision has been prepared in conformance with the Subdivision Ordinance and Subdivision Map Act. A legal description will be prepared and recorded.
That pursuant to Article 7 of the Watsonville Municipal Code, provision has been made for payment of fees or some other provision has been agreed upon by the applicant in the school district to mitigate conditions of overcrowding within that attendance area.	Fees have been paid.
That the facilities to be constructed, purchased, leased, or rented from such fees are consistent with the General Plan.	The proposed auto retail use is consistent with the, zoning and General Plan designations, with approval of a Use Permit by the Planning Commission.

EXHIBIT A
Page 2 of 2

**CITY OF WATSONVILLE
MINOR LAND DIVISION**

EXHIBIT B

Minor Land Division No.: PP2014-165
APN: 017-641-09
Location: 17 West Lake Avenue
Applicant: Mitch Bramlitt, AutoZone Corp.
Hearing Date: September 2, 2014

MINOR LAND DIVISION FINAL CONDITIONS OF APPROVAL

General Conditions Minor Land Division:

1. This Minor Land Division approval shall be null and void if not acted upon within 24 months from the effective date of the approval thereof. Time extensions may be granted pursuant to the Subdivision Map Act provided the applicant requests it prior to expiration of the approval. (CDD-P)
1. This Minor Land Division approval shall not be final until after the ten-day appeal period. In the event of an appeal, issuance of this approval shall be withheld until after the final determination. (CDD-P)
2. This approval applies to the Minor Land Division defined by Legal Description "Tentative Parcel map for AutoZone", received by the Community Development Department on July 14, 2014. The Final Parcel Map shall be in substantial conformance with the approved map unless modified by subsequent conditions of this approval. (CDD-P)
3. The property owner shall agree in writing to identify and defend the City in case of any legal challenge arising out of the City approving the project. Said agreement shall be subject to approval of the City Attorney. (CAT)
4. All solid waste generated inside Watsonville City Limits must be hauled from the site of generation by the City of Watsonville Solid Waste Division as per Watsonville Municipal Code, Chapter 6-3, and City Utilities. This includes all wastes generated at construction sites, excavation projects, land clearing, demolition, earthwork projects, remodels, grading, and tenant improvement projects as well as ongoing business/residential use on the premises. Applicant shall comply with all applicable requirements for removal and disposal of hazardous materials. (PW)
5. The applicant shall: a) print a copy of the conditions of approval on the plans submitted with the Final Map and/or future permits; b) make all revision necessary to comply with conditions of approval; and c) where applicable, note alongside each condition the page number or detail where the condition is satisfied. (CDD-E)
6. The owner shall submit a copy of the mylar documents to the Community Development Department within three days of final action. The CDD Department will arrange for the proper signatures and forward the documents to the County for recording. (CDD-P)
7. Provide a Parcel Map prepared by or under the direction of a licensed land surveyor or

registered civil engineer, prepared in accordance with the Subdivision Map Act. Submit four (4) copies of the Parcel Map, two copies of property boundary closure calculation and one copy of a recent title report. (CDD-E)

8. Prior to Parcel Map recordation, applicant shall enter into an improvement agreement with the City to install public and offsite improvements, furnish securities, insurances and pay the cost of all engineering review and inspection. Said agreement shall be in a form acceptable to the City Attorney. Subdivider shall provide an itemized estimate of the cost of construction of all public and common improvements, and sewer laterals. The cost estimate shall be approved by the City and used to establish the amount of the Securities. (CDD-E)
9. All public and private improvements shall comply with the current City of Watsonville Public Improvement Standards. Plans and design shall be signed and stamped by a California Licensed Architect or Registered Civil Engineer. (CDD-E)
10. All public improvements shall be designed in accordance with the Americans with Disabilities Act. (CDD-E)
11. **POST CONSTRUCTION HYDROMODIFICATION MITIGATION** - The project applicant shall meet the requirements of the City of Watsonville Low Impact Development Best Management Practice Design Guidance for Hydromodification Management and Storm Water Quality for the planning, designing and construction of post construction hydromodification and storm water quality management devices.
12. **STORM DRAIN SYSTEMS/BMP MAINTENANCE AGREEMENT** – The project applicant shall execute an agreement in the standard form providing for the maintenance, monitoring and reporting for the storm drain systems best management practice devices. (CDD-E)
13. Install all utility lines and facilities underground within or adjacent to the development. No new overhead services to the property or overhead extensions of main lines shall be permitted. Service plans shall be approved by the respective utility company and the City prior to the recordation of the Final Map. Improvement plans shall show proposed location of utilities, and Final Map shall provide easements for utilities. (CDD-E)
14. Provide a street lighting plan and calculations prepared by a registered electrical engineer. The average minimum illumination levels shall be 0.4 foot-candle. Streetlights shall be designed with shielding to minimize off-site glare. Plans shall show the street light locations, runs and box locations, all sized and end point tie-ins with utility company. Show all offset distances and stations to all boxes and standards. The developer shall install streetlights at developer's cost. Street lighting will be owned and maintained by the City. (CDD-E)
15. The lot shall have sewer lines established in easements shown on the map. The sanitary sewer line within the public street and public sewer easement shall be publicly, owned and maintained.(CDD-E)
16. Any relocation, rearrangement, or change to existing public facilities due to this development shall be at the developer's expense. (CDD-E)

17. Applicant shall be responsible for repairing or reconstructing any damage to street improvements and asphalt paving on East Lake Avenue. (CDD-E)

Prior to obtaining permits for public improvements and improvement permits, the following revisions must be completed:

18. Submit a preliminary drainage plan, including drainage map, hydrologic and hydraulic calculations signed and stamped by a Civil Engineer. Improvement plans shall provide calculations for the sizing of all drainage improvements, including but not limited to storm drain piping, swales, and ponds. Drainage system shall be designed to convey on-site and contributory drainage from a 10-year storm and shall be designed such that flows from a 100-year storm won't cause adverse conditions such as flooding of neighboring properties. (CDD-E)
19. Provide Best Management Practices (BMPs). Implement the City of Watsonville Best Management Practices (BMPs) to limit and/or reduce pollutants in runoff from roofs, yards, driveways, parking areas and streets. (CDD-E)
20. The soils engineer of record for conformance MUST review all mitigation measures and design recommendations identified in the soils report, approved by the Building Official. Structural foundation design plans must also be reviewed by the soils Engineer of record for conformance to recommendations contained within the soils report. (CDD-B)
21. Prior to requesting a Building Department foundation inspection, the soils engineer shall inspect and approve the mitigation measures and the foundation excavations. The soils engineer shall submit documentation to the Building Division, which verifies compliance with recommendations specified in the soils report. (CDD-B)
22. Prior to or concurrent with the issuance of any building permits, the owner shall pay the Pajaro Valley Unified School District the appropriate fee to address the impact of the additional students. Such fees shall be in conformance with adopted State law in effect prior to approval of the project. Applicant shall submit evidence of payment for each building. (CDD-B, CDD-E)
23. Prior to or concurrent with the issuance of any building permits, the applicant shall pay all development related and impact fees as specified by the City's fee ordinances and identified in the WMC. Additionally, the project shall be subject to the Pajaro Valley Water Management Agency (PVWMA) impact fee ordinance in at the time of building permit issuance. (CDD-E, CDD-B)
24. Prior to issuance of a building permit or the commencement of any site work, the project applicant and the general contractor shall attend a pre-construction meeting with the Building Official and City staff to discuss the project conditions of approval, working hours, site maintenance and other construction matters. The general contractor shall acknowledge that he/she has read and understands the project conditions of approval, particularly those pertaining to construction practices and site safety, and will make certain that all project sub-contractors have read and understand them prior to commencing work and that a copy of the project conditions of approval will be posted on site at all times

during construction. (CDD-E, CDD-B)

25. Prior to permit issuance, applicant shall execute an Engineering Testing and Inspection Agreement and submit to the City for approval. Applicant shall hire a testing firm to perform engineering testing and inspection, such as soils and concrete testing and inspection. The applicant may hire only those firms that are listed on the City's Special Inspection Agency Recognition List. The testing and inspection shall be done at the direction of the City Inspector. The firm shall report nonconforming items to the City Inspector and furnish daily, weekly and final reports as outlined in the agreement and directed by the City Inspector. (CDD-E)
26. No work for which a building permit is required shall be performed within the hours of 7 p.m. to 7 a.m. Monday through Friday, nor prior to 8 a.m. or after 5 p.m. on Saturday, or prior to Noon or after 4 p.m. on Sundays. A sign shall be posted at a conspicuous location near the main entry to the site, prominently displaying these hour restrictions. (CDD-B)
27. Submit an address assignment application, and show mailbox locations on building plans. (CDD-E)

Key to Department Responsibilities

CDD-P	Community Development Department – Planning
CDD-B	Community Development Department – Building
CDD-E	Community Development Department – Engineering
PW	Public Works & Utilities Department
FIRE	Fire Department
CAT	City Attorney

EXHIBIT B
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**CITY OF WATSONVILLE
SPECIAL USE /DESIGN REVIEW PERMIT**

EXHIBITS C & D

Application No. PP2014-165
APN: 017-641-09
Applicant: Mitch Bramlitt, AutoZone Corp.
Approval Date: September 2, 2014

DESIGN REVIEW/SPECIAL USE PERMIT FINDINGS (Section 14-12.403, 513)

1. The proposed use at the specified location is consistent with the policies of the General Plan and the general purpose and intent of the applicable district regulations.

Supportive Evidence

The proposed project is consistent with the CCA (Central Commercial Core) zoning district regulations and meets all of the requirements of the Zoning Ordinance for the site.

2. The proposed use is compatible with, and preserves the character and integrity of adjacent development and neighborhoods, and includes improvements or modifications either onsite or within the public rights-of-way to mitigate development-related adverse impacts such as traffic, noise, odors, visual nuisances, or other similar adverse effects to adjacent development and neighborhoods. These improvements or modifications may include but shall not be limited to the placement or orientation of buildings and entryways, parking areas, buffer yards, and addition of landscaping, walls, or both, to mitigate such impacts.

Supportive Evidence

General architectural considerations are consistent with all design standards including materials, colors, and general design. The structure in the project has been designed according to the same architectural program and will enhance the appearance of the parcel. The project has been determined to be consistent with the architectural guidelines for the downtown.

3. The proposed use will not generate pedestrian or vehicular traffic that will be hazardous or conflict with the existing and anticipated traffic in the neighborhood.

Supportive Evidence

The project will not modify existing driveways or internal circulation to accommodate anticipated traffic on the site and will not conflict with the surrounding uses.

4. The proposed use incorporates roadway improvements, traffic control devices or mechanisms, or access restrictions to control traffic flow or divert traffic as needed to reduce or eliminate development impacts on surrounding neighborhood streets.

Supportive Evidence

The project will maintain the existing circulation facilities that will not be impacted by the new building.

5. The proposed use incorporates features to minimize adverse effects, including visual impacts and noise of the proposed special use on adjacent properties.

Supportive Evidence

The project has existing landscaping that will be generally maintained as part of the project. New landscaping will be required along the street. A landscape plan will be provided as part of the building permit application.

6. The proposed special use complies with all additional standards imposed on it by the particular provisions of this chapter and all other requirements of this title applicable to the proposed special use and uses within the applicable base-zoning district.

Supportive Evidence

The proposed project is consistent with the CCA (Central Commercial Core) zoning district regulations and meets all of the requirements of the Zoning Ordinance for the site.

7. The proposed special use will not be materially detrimental to the public health, safety, convenience and welfare, and will not result in material damage or prejudice to other property in the vicinity.

Supportive Evidence

The project as designed will not be detrimental to public health, safety, convenience or welfare, and will not damage other property in the vicinity.

EXHIBIT C & D
Page 2 of 2

**CITY OF WATSONVILLE
DESIGN REVIEW PERMIT**

EXHIBIT E

Application No.: PP2014-165
APN: 017-641-09
Location: 17 West Lake Avenue
Applicant: Mitch Bramlitt, AutoZone Corp.
Hearing Date: September 2, 2014

General Conditions Design Review:

1. This Design Review shall be null and void if not acted upon within 24 months from the effective date of the approval thereof. Time extensions may be granted provided the applicant requests same at least thirty (30) days in advance of the expiration of this Design Review Permit. This approval applied to the plans titled "Auto Zone" received by the Community Development Department on July 14, 2014. Extensions shall be consistent with that outline in the Specific Plan(CDD-P)
2. After approval is granted, modifications to the project or to conditions imposed may be considered in accordance with Chapter 14-12.1000 of the Watsonville Municipal Code. (CDD-P)
3. Approval is subject to making findings and supportive evidence in accordance with Section 14-12.402 of the City Zoning Ordinance, with attached said Findings, and made a part of this Administrative Design Review Permit. (CDD-P)
4. This approval shall be effective fourteen (14) days after the date of approval. In the event of an appeal, the effective date of this approval shall be withheld until after the final determination thereof by the Planning Commission. (CDD-P)
5. The project shall be in compliance with the conditions of approval, all local codes and ordinances, appropriate development standards and current City policies. Any deviation will be grounds for review by the City and may possibly result in revocation of the Design Review Permit or other Code Enforcement Action. (CDD-P)
6. A copy of the final conditions of approval must be printed on the front sheet of plans submitted for future permits. **Plans without the conditions of approval printed directly on the front page will not be accepted at the plan check phase.** (CDD-P)

Prior to or concurrent with the submittal of a Building Permit application, revise plans to show the following:

7. Unless otherwise noted, City of Watsonville Public Improvement Standards shall be used for private as well as public improvements. All development shall comply with the City of Watsonville Public Improvement Standards. Plans and design documents shall be signed and stamped by a California Licensed Architect or Engineer. Standards that are different than those of the City must be approved by the City. City Standards shall be included in the plans. (CDD-E)

8. An Erosion Control Plan shall be submitted with the permit application. Erosion control plans shall provide Best Management Practices (BMPs) during construction to prevent erosion of constructed slopes, and sediment and contaminants from being entrained in runoff. BMPs shall comply with the City of Watsonville Erosion Control Standards and the Erosion and Sediment Control Field Manual by the California Regional Water Quality Control Board, San Francisco Region, latest edition. All erosion control shall be installed prior to October 15 and be maintained in place at least until April 15. The applicant shall ensure that all contractors are aware of all erosion control standards and BMPs. (CDD-E)
9. The project applicant shall execute an agreement in the standard form providing for the maintenance, and monitoring and reporting of those activities to the City of Watsonville storm drain systems best management practice measures. (CDD-E)
10. Project shall comply with the City's Storm Water Management Plan and NPDES Best Management Practices. Project shall develop storm water management improvements in substantial conformance with conceptual plans. In order to prevent a significant impact occurring, the project drainage must be designed to meet City Best Management practices, NPDES standards, public works drainage standards, and Low Impact Development (LID) standards. (CDD-E)
11. Prior to permit issuance, applicant shall execute an Engineering Testing and Inspection Agreement and submit to the City for approval. Applicant shall hire a testing firm to perform engineering testing and inspection, such as soils and concrete testing and inspection. Testing firms shall be organized, directed and under the supervision of a registered engineer. The testing and inspection shall be done at the direction of the City Inspector. The testing firm shall report nonconforming items to the City Inspector and furnish daily, weekly and final reports as outlined in the agreement and directed by the City Inspector. (CDD-E)
12. Submit a report detailing a comprehensive investigation of surface and subsurface soil and geotechnical conditions prepared by a registered civil or geotechnical engineer. The report and investigation shall address seismic hazards. The report shall determine soil properties and strengths to allow for design of retaining walls, foundations and pavements. In addition, the soils report shall determine the permeability of on-site soils. (CDD-E)
13. On the plans, show the location of proposed mailboxes. Provide written approval of mailbox locations from the US Postal Service. (CDD-P, E)
14. Separate On/Off Site Permits are required for work in the public right-of-way. (CDD-E)
15. Trash enclosures shall be located and designed subject to the review and approval of the City of Watsonville. The City of Watsonville provides front load service for dumpsters and rear load service for drop boxes and compactors. Enclosures shall be sized to accommodate the number of refuse and recycling containers required to store refuse and recycling generation for one week. The design of trash enclosures shall follow, in general, City Standard Drawings Nos. S-809A, S-809B, S-809C and S-810. (CDD-E)
16. Provide landscape and irrigation plans indicating types, quantities, locations and sizes of all plant material including existing major vegetation designated to remain, street trees, method of protecting planting areas from vehicular traffic. Landscape areas shall be incorporated into

storm drainage best management practice measures and therefore the plant materials selected must include appropriate ground covers which filter sediment and pollutants and be tolerant of occasional inundation. All landscaping and irrigation shall be installed and approved prior to occupancy of the project. (CDD-E, P)

17. Three copies of the final landscape and irrigation plans must be submitted and approved by the Community Development Department. The landscape plans shall be coordinated with any bioswales throughout the project. The project shall utilize drought tolerant species, street trees, planter areas, common areas and public right-of-ways. All trees shall be a minimum size of 15 gallons with a minimum spread of four-to-five feet, and one inch caliper trunks. All of the street trees, and trees along the private driveway, shall be 24-inch box specimens. A minimum of 25% of the shrub material shall be a minimum five-gallon size. Automatic, low-flow irrigation systems shall be installed in all landscaped areas. Irrigation is to be programmed for night or early morning hours in order to minimize evaporation. The landscaping plan for all public areas shall be reviewed and approved by the Community and Parks and Recreation Departments for conformance with the Development Plans and this Condition. Additional buffer trees shall be installed along the rear property line adjacent to the Sea View Ranch retaining wall.(CDD-P, PK)
18. In order to achieve the maximum extent possible standard of minimizing hydro-modification, landscape and open-space areas shall be incorporated into the storm drain best management practice measures by means of Low Impact Design. Landscape areas shall be designed with appropriate ground cover to filter pollutants and prevent erosion, and be graded to promote low runoff velocities and enhance absorption into surrounding soils. (CDD-E)
19. All development shall utilize water conservation, water recycling, and xeriscaping to the maximum extent possible. Irrigation systems shall be designed and maintained to avoid run-off, over-spray, or other similar conditions where water flows to waste. Turf shall not be used in median strips, parking islands, or in areas less than eight (8) feet wide, or on slopes that will result in excess irrigation water run-off. (CDD-P, E)
20. In order to reduce the impact of soil erosion or the loss of topsoil to a less than significant impact, the finished ground surface of the project will be planted with ground cover and continually maintained to minimize surface erosion. Final grading and landscaping must not obstruct the site drainage or allow moisture to accumulate adjacent to foundations, slabs, pavements, or other improvements. (CDD-E, CDD-P)
21. The locations of surface mounted utility facilities such as pedestals, transformers backflow devices and fire services shall be planned so that they may be screened utilizing landscaping or other acceptable, visually pleasing means subject to the review and approval of the City of Watsonville. (CDD-E, P)
22. Prior to issuance of a building permit or the commencement of any site work, the project applicant and the general contractor shall attend a pre-construction meeting with the Building Official and City staff to discuss the project conditions of approval, working hours, site maintenance and other construction matters. The general contractor shall acknowledge that he/she has read and understands the project conditions of approval, particularly those pertaining to construction practices and site safety, and will make certain that all project sub-contractors have read and understand them prior to commencing work and that a copy of the

- project conditions of approval will be posted on site at all times during construction. (CDD-B, P, E)
23. Electric and communications services to new buildings shall be constructed underground. Aerial services are prohibited. (CDD-E)
 24. The project will be required to pay Traffic Impact Fees. Fees shall be collected in conformance with standard WMC fee ordinance (CDD-E)
 25. Parking lot layout and parking stall geometrics shall conform to City Standard Plan S-808a. Sidewalks adjacent to perpendicular parking rows shall be sufficiently wide to allow disabled pedestrian access around overhanging cars. Drive aisles shall meet standards for diagonal parking. (CDD-E)
 26. No permanent improvements shall be permitted over easements without written authorization from the easement holder. (CDD-E)
 27. Obtain an encroachment permit for new street improvements driveways and other work in the public right-of-way. The applicant shall be responsible for any repairs within the limits of the development, including streets and paving, curbs and gutters, sidewalks, and street lights, or installation of same where not existing. (CDD-E)
 28. Install all utility pedestals, fire services, backflow prevention devices and equipment with screening to obscure them from view. The materials and design of said screening shall be consistent with regard to design, materials and landscaping of the buildings they serve. (CDD-E)
 29. Plans shall include the location and size of all building utility service connections, including water, gas, electric, fire and irrigation services. Plans shall indicate water service/s size and location and sewer service/s size, type, and slope. Connections shall be located, sized and screened in such a manner that they have the least possible impact on the design of the building and site. (CDD-E)
 30. Project shall comply with the underground utility provisions of Watsonville Municipal Code Title 7, Chapter 16. (CDD-E)
 31. Water services will require City-approved backflow prevention devices. Backflow prevention devices shall be located within 5 feet of the water meters and shall be adequately screened. (CDD-E, P)
 32. Solid waste generated during the construction shall be serviced by the City of Watsonville Solid Waste Division. Applicant shall submit a Solid Waste Service Plan on the City form. (CDD-E, PW)
 33. In order to reduce the impact of strong seismic ground shaking to a less than significant impact the project shall comply with the 2013 building code as it relates to seismic design and the recommendations of the Geotechnical report. (CDD-E, CDD-B)

34. Improvements (new framing, electrical, mechanical, plumbing) Obtain all required building permits (Building, Plumbing, Mechanical, Grading etc.) for this project. All construction shall comply with all State Building Codes; Framing, mechanical, plumbing, electrical, T-24 energy, T-24 Accessibility and Municipal codes in effect at the time of plan submittal for building permits resulting in actual construction. (CDD-B)
- a. A design professional will be required at time of construction drawings, to prepare plans for proposed improvements per the business and professions code
 - b. Comprehensive detailed construction plans are required at the time of submittal to be reviewed for adequate content prior to intake, by the Building Official
 - c. Provide Occupancy group type of construction proposed per the CBC, to clearly determine requirements.
 - d. In January 1, 2013 the new California Building Code (CBC) became effective. All construction projects shall comply with the City's Green Building Ordinance and all amended California Building Codes including:
 - 2013 California Building Code
 - 2013 California Electrical Code
 - 2013 California Mechanical Code
 - 2013 California Plumbing Code
 - 2013 California Energy Code
 - 2013 California Fire Code
35. Prior to excavation, adjoining landowners shall be given notice of the date, location, and extent of excavation in conformity with Section 832 of the Civil Code and copies shall be provided to the Building Official prior to issuance of the Building Permit. (CDD-B)
36. Prior to requesting a Building Department foundation inspection, the soils engineer shall inspect and approve the mitigation measures and the foundation excavations. The soils engineer shall submit documentation to the Building Division which verifies compliance with the recommendations specified in the soils report. (CDD-B)
37. As indicated by CBC Section 1704, the owner or the engineer or architect of record acting as the owner's agent shall employ one or more special inspectors who shall provide special inspections when required by CBC Section 1704. Please contact the Building Division at time of plan submittal to obtain application for special inspections. (CDD-B)
38. Provide Structural Calculations verifying compliance with all applicable provisions of the California Uniform Building code Chapter 16. Prior to request for final inspection, written verification by the engineer of record indicating compliance with the structural design shall be submitted to the City of Watsonville Building Division. (CDD-B)
39. Comply with all applicable provisions of the California State Building Code (Title 24) Part 2 Chapter 11B Division I, II & III for Disabled Access. Plans must show compliance in sufficient information and detail to determine compliance was noted for the following:
- a. Path of travel from Public Transportation (main entry to the public sidewalk).
 - b. Disabled parking requirements:
 1. Van Accessible Parking (requires 8'-0" unloading area)
 2. Number of spaces (1 for 1st 25, 2 for the next 50 see table 11B-6).

3. Path of travel from accessible parking to any elevators
 4. Slopes at parking & unloading areas must not exceed 1:50
 5. Proper disabled signage, lettering and stripping is required (CDD-B)
40. Main building entrances and required exits must be accessible. Design professional must provide written verification of compliance for existing disabled access features or facilities noted on plans. (CDD-B)
 41. The building shall have Automatic Fire Sprinklers installed, complying with NFPA installation standards. The fire sprinkler contractor shall submit three sets of plans and calculations for a separate fire permit prior to installation of the system. (CDD-B, WFD)
 42. The project shall comply with the current CBC, and CFC 2013 regulations for fire issues. (CDD-B, WFD)
 43. A UL central station shall monitor all fire sprinkler systems. The monitoring shall provide water flow notification to the hearing and visually impaired. The monitoring contractor shall submit three sets of plans for a separate fire permit prior to installation of the system. (CDD-B, WFD)
 44. The building shall be provided with the required size and number of fire extinguishers. Exterior doors providing access to the fire risers and alarm panel shall have proper signage installed. (CDD-B, WFD)
 45. Civil drawings shall include the proposed location of the fire department connection, above ground backflow device, post indicator valve, and size of fire service needed for this project (CDD-B, WFD).
 46. Adequate water for firefighting shall be available prior to combustible construction. (CDD-B, WFD)
 47. In order to prevent an adverse impact from exterior light sources, a final lighting plan will be required to be submitted at the time of building permit application to ensure that the location, height, and angle of all exterior lighting will not interfere with adjacent properties including the neighboring airport. (CDD-P)
 48. Provide a detailed color and Materials Board. A copy of the paint colors for the project with indication of location of each color (color board), shall be submitted for review and approval by the Community Development Department. (CDD-P)

During construction, the following conditions shall be adhered to:

General Conditions:

49. Provide Best Management Practices (BMPs) during construction to prevent sediment, debris and contaminants from draining offsite. BMPs shall comply with the City of Watsonville Erosion Control Standards and the Erosion and Sediment Control Field Manual by the California Regional Water Quality Control Board, San Francisco Region, latest edition. All erosion control shall be installed prior to October 15 and be maintained in place until April 15. Provide a note on the improvement plans stating that construction should take place between April 15 and October 15. The applicant shall ensure that all contractors are aware of all erosion

control standards and BMPs. (CDD-E)

Construction Notes to be included with the Improvement Plans:

50. Existing public facilities damaged during the course of construction or in an existing state of disrepair shall be repaired by the applicant, at the applicant's expense, to the satisfaction of the City. (CDD-E)
51. Applicant shall have onsite at all times, a superintendent that shall act as the owners' representative and as a point of contact for the City's Public Works Inspector. The superintendent shall be authorized by the Owner to direct the work of all contractors doing work on public and private improvements. (CDD-E, PW)
52. Contractor shall provide a minimum of 48 hours notice in advance of any required inspection. Any temporary suspension of work or returning to work for any reason shall be cause for the developer or contractor to telephone the Public Works Inspector at 768-3100. (CDD-E)
53. Prior to excavation, contractor shall locate all existing underground utilities. Call Underground Service Alert (U.S.A.) at 1-800-642-2444 to have utilities located and marked in the field. (CDD-E)

Prior to Final Occupancy:

54. Prior to issuance of the certificate of occupancy, the project designers including civil, structural, and geotechnical engineers shall provide statements of compliance attesting that they have reviewed the completed project and that it was constructed in conformance with their recommendations and plans. (CDD-E)
55. As-built plans shall be provided for the project in the following manner:
 - a. The applicant's contractor shall maintain one set of full size, approved plans and mark thereon any deviations from plan dimensions, elevations or orientations. Marked plans shall be updated weekly and shall be available to the City for review when requested. Revisions to the plans shall be done in black ink. They shall be clouded and a delta or a note placed next to the clouding that indicates that the change was done as the plans were being "as-built." As-built plans shall be maintained for all approved improvement plans, including but not limited to, grading, retaining wall, drainage, utility, roadway, landscape and irrigation plans.
56. Prior to final City acceptance of the project, all design professionals who prepared improvement plans for the project (civil, geotechnical, electrical and structural engineers), shall provide letters attesting that they have periodically monitored the construction and have reviewed the completed work and that it was constructed in substantial conformance with their plans and recommendations. Where special inspections and testing were involved, the letters of compliance shall be accompanied by inspection logs, testing and analysis that support the engineer's conclusions. (CDD-E)

EXHIBIT E
Page 7 of 8

Ongoing Conditions:

- 57. Landscaping and site improvements shall be maintained in perpetuity. Landscaping shall be maintained by a professional landscape maintenance company. All dying and/or diseased vegetation shall be immediately replaced in kind. (CDD-P)
- 58. No non operable vehicles shall be allowed to be stored on site. (CDD-P)

Key to Department Responsibilities

CDD-P	Community Development Department – Planning
CDD-B	Community Development Department – Building
CDD-E	Community Development Department – Engineering
PW	Public Works & Utilities Department
FIRE	Fire Department
CAT	City Attorney

EXHIBIT E
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**CITY OF WATSONVILLE
PLANNING COMMISSION**

EXHIBIT F

Application No. PP2014-165

APN: 017-641-09

Applicant: Mitch Bramlitt, AutoZone, Corp.

Hearing Date: September 2, 2014

Applicant: Mitch Bramlitt

Property Owner: William Hansen

Address: 123 South Front Street, Memphis, TN, 38103

Project: Minor Land Division with Special Use and Design Review Permit with Environmental Review (PP2014-165)

Location: 17 West Lake Avenue, Watsonville, CA 95076

Purpose: Minor Land Division to create a 10,193 sq. ft. parcel and Special Use and Design Review Permit with Environmental Review to allow construction of a two story 8,973 sq. ft. for an auto supply store for AutoZone Corporation.

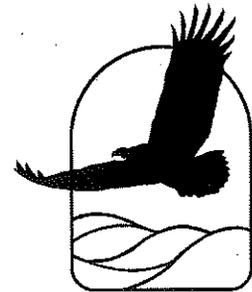
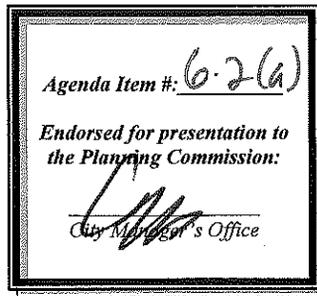
The Minor Land Division, Special Use and Design Review Permit with Environmental Review Application (PP2014-165) requested by the applicant to allow the creation of a 10,193 sq. ft. lot for the construction of a two story 8,973 sq. ft. auto supply store at 17 West Lake Avenue, was reviewed by the Planning Commission at a public hearing on September 2, 2014 and was conditionally approved by adoption of Planning Commission Resolution No. _____ (PC) together with findings and conditions of approval attached hereto and made a part of this permit.

**CITY OF WATSONVILLE
Planning Commission**

Marcela Tavantzis
Community Development Director

EXHIBIT F
Page 1 of 1

6.2(a)



City of Watsonville
MEMORANDUM

Date: August 14, 2014

To: Planning Commission

From: Marcela Tavantzis, Community Development Director
Keith Boyle, Principal Planner

Subject: CONSIDERATION OF APPLICATION (PP2014-175) TO RECOMMEND TO THE CITY COUNCIL ON A REZONING FROM INSTITUTIONAL ZONING (N) TO CENTRAL COMMERCIAL (CC) FOR PARCEL A.P.N. 017-183-04 WITH CONSIDERATION OF A TEXT AMENDMENT FOR ALLOWANCE OF DRIVE-THROUGHS WITH A USE PERMIT IN THE CC ZONE. ADDITIONALLY, CONSIDER RECOMMENDATION ON SPECIAL USE PERMIT WITH DESIGN REVIEW AND ENVIRONMENTAL REVIEW (PP2014-117), TO CONSTRUCT A 4,278 SQ. FT. 24 HOUR FAST FOOD RESTAURANT WITH A DRIVE-THROUGH FACILITY (MCDONALD'S), AT 174, 186, & 190 MAIN STREET (APN'S: 017-183- 04, 05 &10); INCLUDING A VARIANCE TO REDUCE THE DISTANCE OF THE DRIVE-THROUGH FROM 200 FEET TO 160 FEET, FROM A SIGNALIZED INTERSECTION SUBJECT TO COUNCIL ACTION ON THE REZONING AND TEXT AMENDMENT .

Agenda Item: September 2, 2014

Planning Commission

RECOMMENDATION:

Staff recommends the Planning Commission adopt a Resolution recommending that the City Council:

- Adopt an Ordinance Text Amendment to the CC zoning district (Watsonville Municipal Code (WMC) Section 14-16.1104 (b)) to allow drive-throughs with a Special Use Permit.
- Adopt a Map Amendment Ordinance re-designating the property at 190 Main Street (APN: 017-183-04) from Institutional (I) to Central Commercial (CC).
- Adopt a Resolution approving the Special Use Permit with Design Review for the construction of a 4,278 sq. ft. fast food restaurant with a Variance (WMC Section 14-41.100 (a) (2)) to allow a drive-through within 160 feet from a signalized intersection.

BASIC PROJECT DATA

APPLICATION NO. PP2014-175 - Rezoning and Text Amendment
PP2014-117 - Special Use Permit, Design Review, Variance, Environmental Review

LOCATION: 174,186 &190 Main Street **APN'S:** 017-183-04, 05 & 10

SCOPE OF PROJECT: A rezoning from N to CC for Parcel APN: 017-183-04; text amendment to WMC Section 14-16.1104 (b) allowing drive-through facilities with a Special Use Permit in the Central Commercial Zone (CC). Special Use Permit with Design Review and Environmental Review, to construct a 24 hour 4,278 sq. ft. fast food restaurant including a Variance to reduce the drive-through distance from a signalized intersection from 200 feet to 160 feet.

GENERAL PLAN: Neighborhood Mixed Use

ZONING: CC (Commercial Core) N (Institutional)
ADJACENT GP/ Zoning: Public Facilities and Commercial Core

EXISTING LAND USE: Vacant parking lot, 10,314 sq. feet of existing building area
PROPOSED LAND USE: 24 hour 4,278 sq. ft. fast food restaurant with drive-through facility

FLOOD ZONE: Yes

APPLICANT: Core States Group, 150 West Lake St., Ste. 212 Kirkland, WA 98119
PROPERTY OWNERS: Fred Oda, Lupe Beltran, & City of Watsonville

CALIFORNIA ENVIRONMENTAL QUALITY ACT REVIEW (CEQA): The project is less than 10,000 sq. ft. and qualifies for a Class 15303 Categorical exemption because a traffic study, historic survey, and drainage study found no potential impacts from the development.

BACKGROUND/ DISCUSSION

Proposal: Core States Group is proposing to assemble three parcels (APN's 017-183-04, 05 &10 (174, 186 and 190 Main Street) that total 35,083 square feet and remove the existing buildings totaling 10,314 sq. ft. that occupy the site including a two story, mixed use building at 174 Main Street (5,134 sq. ft.), a one story commercial building at 186 Main Street (3,968 sq. ft.), an abandoned home (1,214 sq. ft.) and a surplus parking lot owned by the City. The applicants are proposing to replace the existing structures with a new 4,278 sq. ft. McDonald's restaurant with 36 parking spaces and a drive-through facility.

The existing buildings are approximately 80 years old and are in a significant state of disrepair. A historic inventory prepared by Anthony Kirk indicates these buildings do not have historic relevance due to major changes to the façade and state of building disrepair. The report documents the buildings' history and this is all that is necessary to

meet the state historic requirements (Attachment 3). During the 50's through 90's, these parcels contained a series of bars that were a significant safety issue during their operation with nightly police calls for service. Core States Development is proposing to replace these marginal buildings with a new family friendly facility that will help attract people to the entrance of Downtown Watsonville and create an inviting atmosphere for residents to enjoy.

Site and Vicinity: The site is located in the Central Commercial Zoning District (CC). It is at the busy intersection of Main Street and Riverside Drive (Highway 129). Presently this intersection has two service stations, a Burger King and El Pollero drive-through restaurant. The proposed McDonald's restaurant is similar in nature to the existing facilities at the intersection including the two restaurants with drive-through facilities, which are also in the CC zoning district. The CC zoning district allows restaurants in the district. The surplus parking lot owned by the City that is included in this project contains 19 parking spaces and a drive aisle from Main Street to Union Street. This parcel also includes 15 parking spaces within the Police lot. The City has agreed to sell this surplus property to the applicants to allow the project to move forward. Selling the site will allow the continued use of the area for parking and circulation for private use, with public access within the McDonald's parking lot.

Background: The City of Watsonville is promoting improvements to the downtown as a high priority to begin updating the image of Watsonville. The proposed project is a highly visible intersection in the community and will benefit from update of the existing building configuration with a family friendly restaurant. In 2000, the City adopted a Drive-Through Ordinance that established design criteria for new drive-through facilities. A text amendment will be included as part of this project to recognize that the CC zone is an appropriate location for drive-through restaurants with a Use Permit based on the location of several drive-through restaurants currently in the zone.

PROCEDURE

Rezoning:

Two of the subject properties are currently zoned Central Commercial (CC) with the adjacent City parking lot being zoned Institutional (N). The 2030 General Plan designation for all of the properties are Neighborhood Commercial Mixed Use. It was anticipated that this surplus lot could support new commercial uses, and the General Plan designation was changed to reflect this. The applicant has submitted an application to update the zoning on the surplus parking site to match the proposed commercial site, in order to establish a consistent zoning for the entire site. This would change the zoning for the City lot from Institutional (N) to Commercial Core (CC). Staff is supportive of the proposed rezoning to allow the lots to be consolidated with a single zoning designation of CC.

The Findings for recommending approval of the rezoning are attached as Exhibit A to the Resolution.

Text Amendment:

Additionally, the City has initiated a text amendment clarifying that drive-through facilities are allowed in the CC zone with a Use Permit. When the Drive-Through Ordinance was adopted in 2000, there was a desire to limit drive-through facilities to existing areas of town that had drive-through facilities. It appears that there was an oversight to not allow drive-through restaurants in the CC district. The CC district is currently a small area near the City's downtown central core. Since there are currently several drive-through facilities in the vicinity, it appears that this proposed use would be consistent with the surrounding area. Staff is recommending a minor text amendment to allow drive-through restaurants with a Use Permit in the CC district.

This modification will not have a substantial impact in the limited CC district area but may allow similar uses to be established to support changes at the entrance to downtown and help bring new vitality. Any drive-through facility would be required to have a public hearing to determine the project's compatibility with adjacent uses and consistency with drive-through regulations.

The Findings for recommending approval of the text amendment are attached as Exhibit B to the Resolution.

The Planning Commission makes recommendations to the City Council on both text amendments and zoning map amendments. Since these modifications would be necessary to fully support the proposal, staff is recommending that the Special Use Permit and Design Review applications for the project also be forwarded to the City Council with a recommendation that they be considered depending on the final action on the map and text amendments taken by the City Council.

Special Use Permit and Design Review:

Section 14-16.1102 of the WMC allows restaurants as a permitted Use with drive-through facilities listed as an exclusion. The proposed text amendment will clarify that drive-through restaurants are allowed in the CC zone with a Use Permit. The Planning Commission is being requested to recommend the text amendment and the project to the City Council for final action. Since there are several other drive-through restaurants in the CC zoning district, the use appears to be consistent with the intent of the zoning regulations.

Per Section 14-12.400 of the WMC, commercial, and industrial, construction requires approval of a Design Review Permit process acceptable to the Zoning Administrator. As proposed, the Special Use Permit and Design Review Permit are being processed concurrently with the text and map amendment requests, and are all being forwarded to City Council for final action.

TECHNICAL ANALYSIS

Zoning, General Plan: The site is zoned CC (Central Commercial) with a Neighborhood Mixed Use Land Use Designation in the 2030 Watsonville: General Plan Map. The CC zone allows restaurants as a permitted use. The City's surplus lot is designated

Institutional which allows parking and circulation that is being proposed for this parcel. Additionally, the project includes rezoning of this parcel to CC to be consistent with the rest of the site.

PROJECT DESIGN

The proposed McDonald's restaurant utilizes a new modern proto type for the restaurant. This new type of building was recently completed at the Overlook Shopping Center and will serve as a good compliment to the entrance to Watsonville's Main Street. The building will include stucco front with a corrugated metal accent band across the entire roof mansard. The building includes a variety of trellises and paint colors to break up the massing and a modern stylized arch that has been included on three elevations that is consistent with the new building proto type. Building entrances will be off of Main Street and the parking lot side of the building. The building will be equipped with fire sprinklers and an automatic fire alarm system, per state requirements.

Access/Circulation:

The project is designed with one entrance off of Main Street and one entrance and one exit off of Union Street. These entrance points generally reflect the current location of the driveways from the existing City lot. There are no new driveways that will disrupt the existing circulation pattern in the area. The Main Street entrance has been expanded to be 28 feet in width and will allow more efficient circulation on site. The driveway will be approximately 160 feet from the corner of Riverside Drive and Main Street. The applicants are requesting the ability to have full movement from this driveway. The detailed Traffic Study (Attachment 4) indicates that movements will not be problematic; however, during the afternoon peak between 4:00 – 6:00 p.m. it will be difficult for any vehicle to turn left from the Main Street driveway.

Traffic: A Traffic Study was prepared for the project by Associated Transportation Engineers on February 5, 2014 (Attachment 4). The intersections in the vicinity operate at a Level of Service (LOS) of D or better during a.m. and p.m. peak hours. Currently the worst existing intersection in the vicinity is the Main Street and Riverside Drive intersection that operates at LOS D during peak p.m. hours. The study indicates that the project could generate approximately 71 new p.m. peak hour trips at this intersection. The study indicates that this additional traffic will not change the LOS of the existing intersection beyond the current LOS, and therefore is not considered a significant impact to traffic.

Drive Through: Section 14-41 of the WMC establishes design standards for drive-through facilities. The proposed drive-through meets all of the standards included in the section including; being setback over 20 feet from the driveway entrance (35 feet provided); having stacking for over 4 vehicles (14 provided); menu boards screened from view; having separate access from internal circulation; and having concrete driveway for the drive-through etc. The only standard that is not met is that the drive-through lane should be 200 feet from a signalized intersection. As designed, the drive-through lane is approximately 160 from Riverside Drive and Main Street which is a signalized intersection. This driveway has been in place for many years. Use by the restaurant will not add a significant burden to the facility as identified in the traffic study. Distance standards were established for drive-through lanes to maintain safety. Based on the use of this facility as an existing

driveway, it appears that safety has been demonstrated over the years and a variance to the provisions is allowable.

The Findings recommending approval of the Variance are attached as Exhibit E to the Resolution.

Parking: A total of 36 parking spaces are proposed to be provided for this site. Section 14-17.801(i) establishes the parking standards for restaurants at 1 space per each 100 sq. ft. of gross floor area. Additionally, Section 14-17.108 allows gross floor area for commercial space to be calculated at 85% of the gross space within the walls. For purposes of calculation, the 4,278 gross floor area times 85% = 3636 sq. ft.; $3636/100 = 36$. Therefore a total of 36 parking spaces is required which is proposed to be provided by the project. The proposed 16 foot driveway widths meet the requirements for one-way circulation with angled parking identified in the City public improvement standards. The sale of the City lot to the project involves the loss of 15 parking spaces from the Police lot. This lot serves the Police as well as some other City staff parking. With the loss of these spaces, only Police associated vehicles will be allowed to park in the lot.

Landscaping: a preliminary landscape plan was submitted with the application that includes approximately 12% of the project site being landscaped. The plan includes a detailed drainage study to meet the new regional water quality design standards. The site includes a mix of drought tolerant shrub and groundcover species throughout the site. A bioswale is proposed to be designed to catch and filter stormwater before it enters the storm drain system, in compliance with stormwater standards. Three copies of the final landscape plan shall be submitted at the time of building permit submittal for review.

Environmental: The project was required to prepare a traffic study, historic inventory, and a drainage study to determine potential impacts of the proposal. Section 15303(c) of the CEQA guidelines allows the use of a Categorical Exemption for commercial projects less than 10,000 sq. ft. if no significant impacts are found. The proposed restaurant is consistent with the allowed uses of the CC zone. The drive aisles and parking on the City lot are also consistent with the allowed uses in the Institutional zone. The proposed rezoning and text amendments are clarifications that are making the project consistent with the surrounding zoning and can also be covered by the exemption.

Healthy Eating Options: In 2010, the City adopted the Healthy Eating Options Ordinance that requires new restaurants to meet minimum healthy eating options to obtain a building permit. The system establishes a series of points for various healthy eating options. A project must indicate that it can achieve 6 points to get a building permit. The proposed McDonald's project meets this criteria as follows: Offers 4 choices of fruits and vegetables (2 points); offers a low-fat salad dressing (1 point); includes skim or 1% milk on menu (1 point); offers water free of charge (1 point); provides training for staff about the menu (1 point); and provides calorie information visible at the point of sale (2 points) for a total of 8 points. Therefore the project is consistent with the City's Healthy Eating Options Ordinance.

SUMMARY

The proposed project will replace two existing buildings that have created significant safety problems over the last 40 years, with a new family friendly facility that will complement the entry to Main Street and help begin to refresh the image of Watsonville. The project is compatible with existing drive-through restaurants in the vicinity and has been designed in a manner that provides safe access and circulation to and from the site. The project as designed will be an asset to the community.

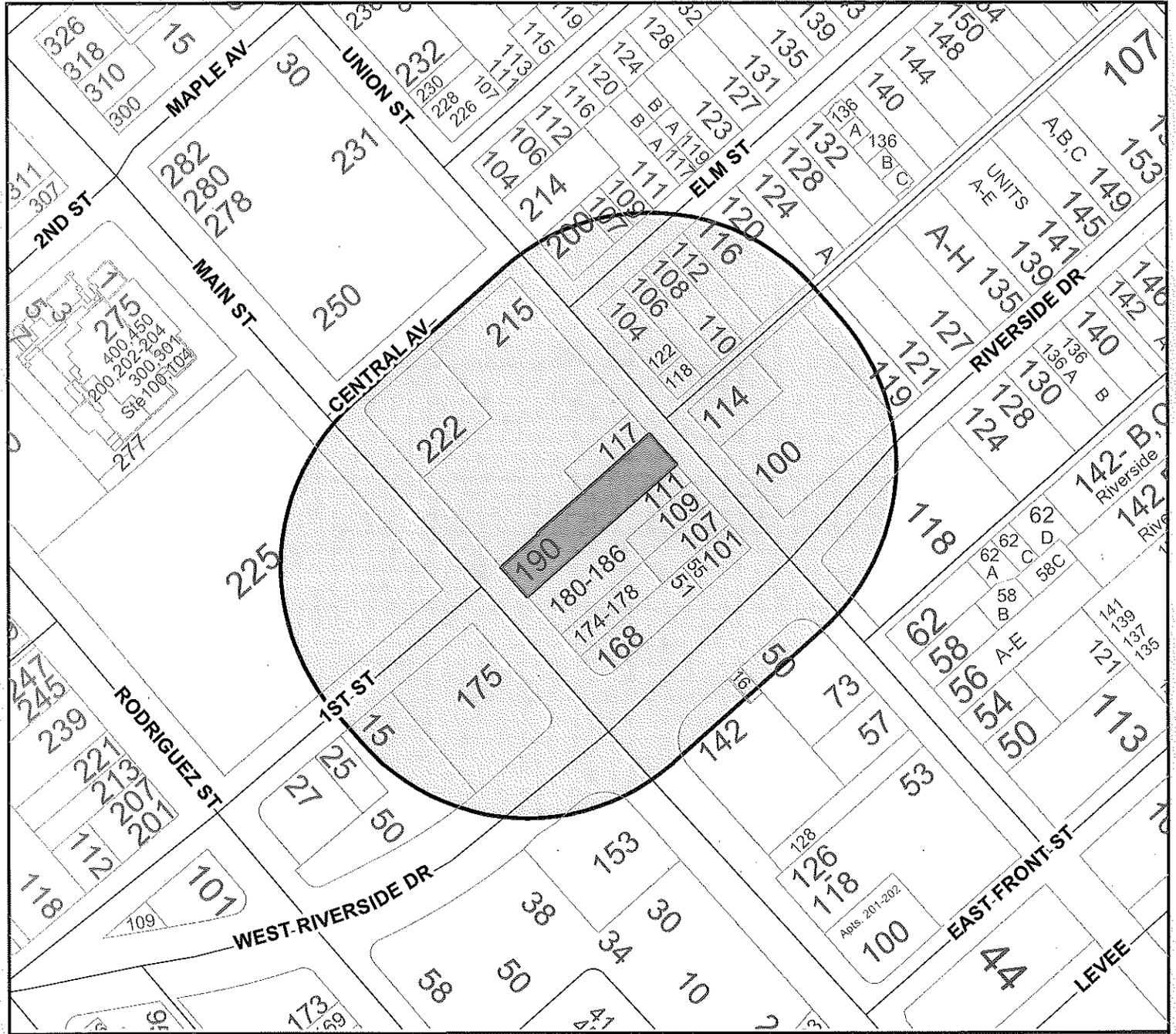
Staff recommends the Planning Commission adopt a Resolution recommending that the City Council:

- Adopt an Ordinance Text Amendment to the CC zoning district (WMC Section (14-16.1104 (b)) to allow drive-throughs with a Special Use Permit.
- Adopt a Map Amendment Ordinance re-designating the property at 190 Main Street (APN: 017-183-04) from Institutional (N) to Central Commercial (CC).
- Adopt a Resolution approving the Special Use Permit with Design Review for the construction of a 4,278 sq. ft. fast food restaurant with a Variance (WMC Section 14-41.100 (a) (2)) to allow a drive-through within 160 feet from a signalized intersection.

ATTACHMENTS

1. Site and Vicinity Map
2. Project Plans
3. Report from Anthony Kirk on Historic Issues
4. Traffic Study
5. Class 3 Categorical Exemption

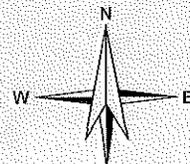
Site and Vicinity Map



Legend

-  Street
-  Parcel
-  300' Buffer Zone
-  Watsonville City Limit
-  Project Site

PROJECT: PP2014-175
 APPLICANT: City of Watsonville
 APN#s: 017-183-04
 LOCATION OF PROJECT: 190 Main St

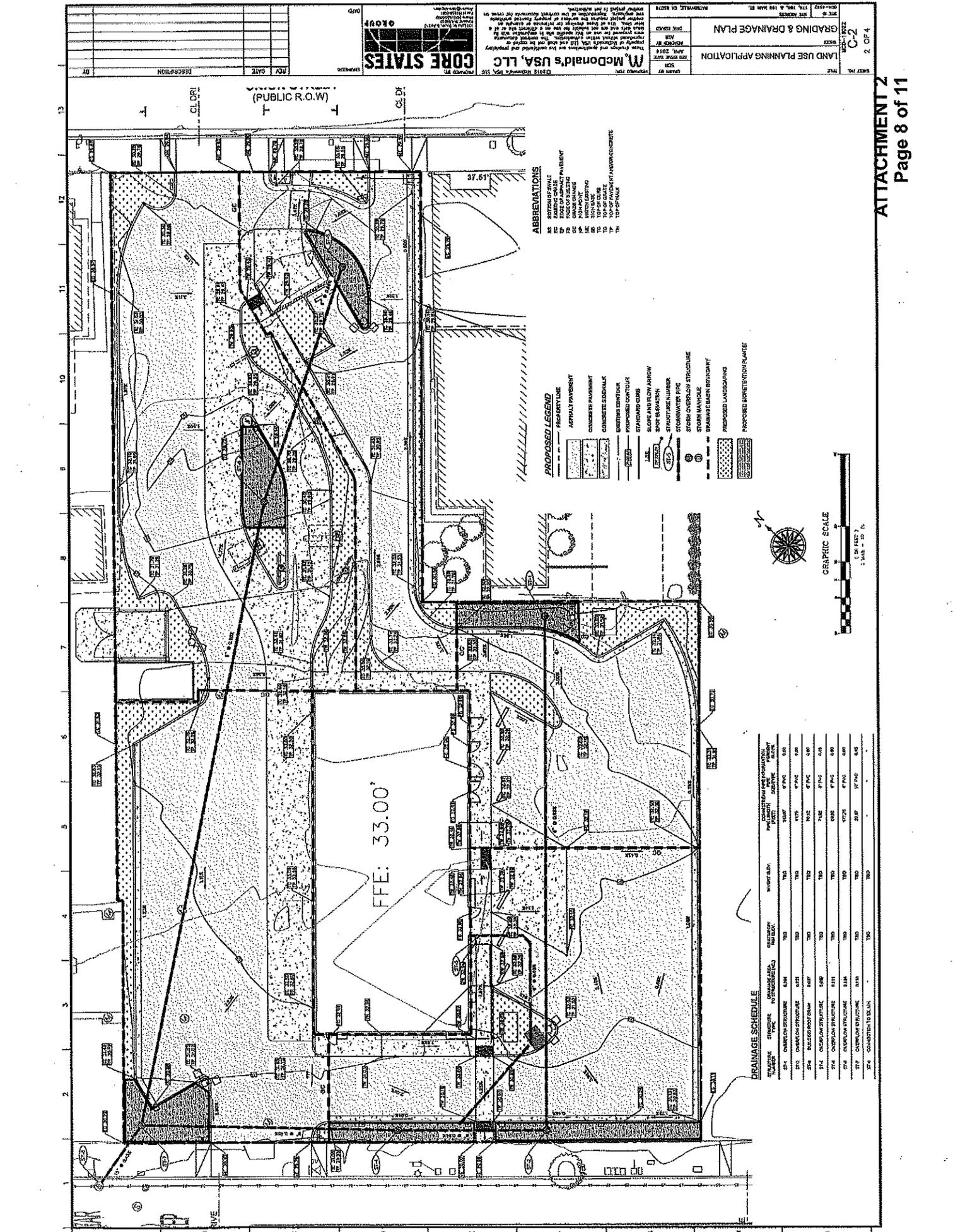


ATTACHMENT 1
 Page 1 of 1



Prepared by Watsonville GIS Center 7/30/2014 (C0DD1447).

This Document is a graphic representation only of best available sources.
 The City of Watsonville assumes no responsibility for any errors.



- ABBREVIATIONS**
- 1. 1" = 10' SCALE
 - 2. 1" = 20' SCALE
 - 3. 1" = 40' SCALE
 - 4. 1" = 80' SCALE
 - 5. 1" = 160' SCALE
 - 6. 1" = 320' SCALE
 - 7. 1" = 640' SCALE
 - 8. 1" = 1280' SCALE
 - 9. 1" = 2560' SCALE
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 - 229. 1" = 43135914670278019021278786213996703986667360' SCALE
 - 230. 1" = 86271829340556038042557572427993407986667680' SCALE
 - 231. 1" = 172543658681112076085115144855986815986667360' SCALE
 - 232. 1" = 34508731736222415217023028971197363986667680' SCALE
 - 233. 1" = 69017463472444830434046057942394727986667360' SCALE
 - 234. 1" = 138034926944889660868092115884789455986667680' SCALE
 - 235. 1" = 276069853889779321736184231769574911986667360' SCALE
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 - 251. 1" = 18092513944520577629302569713208398666667360' SCALE
 - 252. 1" = 36185027889041155258605139426416798666667680' SCALE
 - 25

State of California -- The Resources Agency DEPARTMENT OF PARKS AND RECREATION PRIMARY RECORD	Primary # HRI #
Other Listings Review Code	Trinomial NRHP Status Code
Reviewer	Date

Page 1 of 5

*Resource Name or #: (Assigned by recorder) 174-178 Main Street

P1. Other Identifier:

*P2. Location: Not for Publication Unrestricted *a. County Santa Cruz

and (P2b and P2c or P2d. Attach a Location Map as necessary)

*b. USGS 7.5' Quad Watsonville West Date 1995 T 12 S; R 2 E; Mount Diablo B.M.

c. Address 174-178 Main Street City Watsonville Zip 95076

d. UTM: (Give more than one for large and/or linear resources) Zone ; mE/ mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

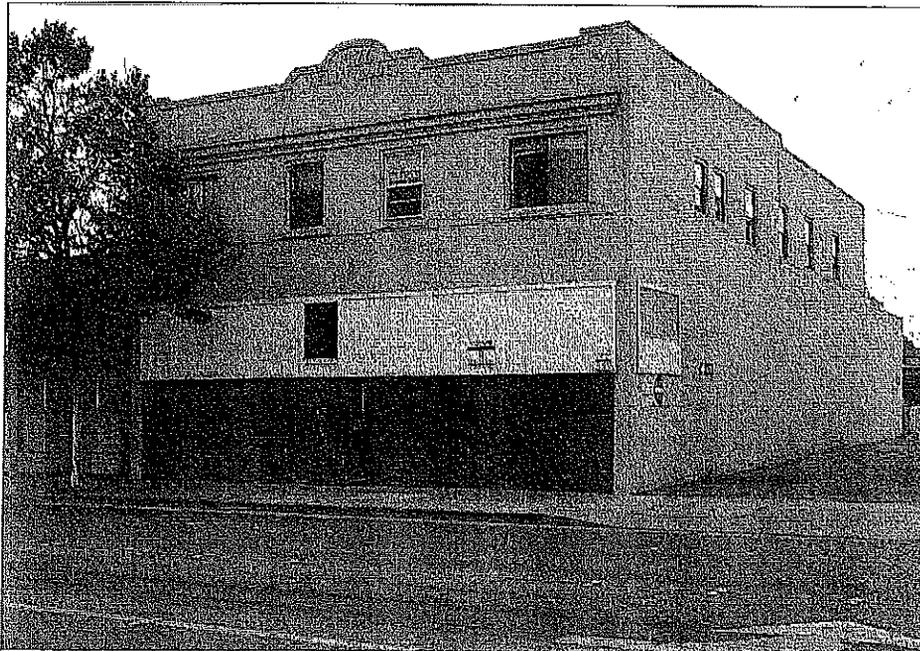
Santa Cruz County APN 017-183-10

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting and boundaries)

The two-story commercial building and adjoining one-story storage block at 174-178 Main Street were constructed in 1926 and subsequently altered in several phases (DPR 523A photo and figures 1 and 2). The reinforced-concrete building and storage block are rectangular in plan, encompassing 4,000 square feet, and rest on a concrete-slab foundation. The ground floor of the building, previously leased for retail use, is currently vacant; the upper-level is a single-room-occupancy (SRO) hotel. A veneer of bright red tile runs along the ground floor of the façade, facing Main Street to the southwest, or west as it is designated in this report for simplicity. Above the tile is a broad band of grooved-face T1-11 siding. The stuccoed façade rises to an ornamental cornice and above it to a shaped parapet wall with molded trim and a relief inscription, 1926 / Kelly Block. Three wooden doors provide access to 174, 176, and 178 Main Street. The doors at either end are in set in canted recessed entrances; the central door, which leads to the hotel, is flush-with the building envelope. The north and south sides of the building have no wall cladding. At the rear of the building, the east side of the storage block and the second floor are clad with corrugated metal sheets. Fenestration is limited on the ground floor to fixed smoked-glass windows flanking the entrances to 174 and 178 Main Street. The windows at (continued on page 3)

*P3b. Resource Attributes: (List attributes and codes) HP5 - Hotel/Motel; HP6 - Commercial Building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)



P5b. Description of Photo: (View, date, accession #) Looking northeast at west and south sides, 7/30/14

*P6. Date Constructed/Age and

Sources: Historic

Prehistoric Both

1926; date block in parapet

*P7. Owner and Address:

Lupe Beltran Elizalde

(address not available)

*P8. Recorded by: (Name, affiliation, and address)

Anthony Kirk, Ph.D.

1140 West Cliff Drive, No. 3

Santa Cruz, CA 95062

*P9. Date Recorded: 8/5/14

*P10. Survey Type: (Describe)

CEQA Survey

*P11. Report Citation: (cite survey report and other sources, or enter "none.") None

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure and Object Record

Archaeological Record District Record Linear Feature Record Milling Station-Record Rock Art Record

Artifact Record Photograph Record Other (List)

ATTACHMENT 3

Page 1 of 10

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 5

*NRHP Status Code

*Resource Name or # (Assigned by recorder) 174-178 Main Street

B1. Historic Name: Kelly Block

B2. Common Name: None

B3. Original Use: Commercial Building; Hotel

B4. Present Use: Ground Floor Vacant; Upper Floor SRO Hotel

*B5. Architectural Style: None

*B6. Construction History: (Construction date, alteration, and date of alterations) Constructed 1926; west side of first floor remodeled ca. 1950s or 1960s and again in 1980s, when windows were probably replaced.

*B7. Moved? No Yes Unknown Date:

Original Location:

*B8. Related Features: None

B9a. Architect: Unknown

b. Builder: Unknown

*B10. Significance: Theme n/a

Area n/a

Period of Significance n/a

Property Type n/a

Applicable Criteria n/a

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Address integrity.)

The commercial building at 174-178 Main Street does not appear to be eligible for the California Register of Historical Resources. The property is part of the former Chinatown that in the 1920s stretched north along Union, Main, and Rodriguez Streets from the Pajaro River to roughly 1st Street. Despite the name, the area was characterized by a scattering of Chinese establishments and far more Japanese businesses, particularly south of Bridge Street (now Highway 129). Close to the river were lodging houses and residences occupied by a sprinkling of Filipinos and many Chinese and Japanese. In 1927, the year the Kelly Block was constructed, the ground floor held the Canton Market and the Sang Fat Company, which offered Oriental merchandise. Nothing is known about the Carlton Hotel, which long operated above the two stores, but given the character of the neighborhood, it presumably catered to budget travelers or possibly served as a monthly rental. With the bombing of Pearl Harbor, the Japanese were forcibly moved to relocation camps and the area slowly became more Hispanic, as Mexican laborers moved north to work in the fields, taking the positions formerly held by the interred Japanese. The Carlton Hotel was condemned in 1955, according to the Commercial Building Record for the property, but it continued to operate in what was increasingly a Hispanic section of town, though in 1960 the Kelly Block still held the Canton Market, as well as Wings Café. The neighborhood was at this date described as "blighted," "with extremely low rents." By the outset of the following decade, the two ground floor retail spaces were occupied by La Frontera, which offered food and drink in a neighborhood that was increasingly (continued on page 3)

B11. Additional Resource Attributes (List attributes and codes):

*B12. References:

Commercial Building Record, 174-178 Main Street, Santa Cruz

County Assessor.

Watsonville City Directories (1927-89)

Sanborn Map Co., *Watsonville, California* (1911, 1920, 1939, 1962).

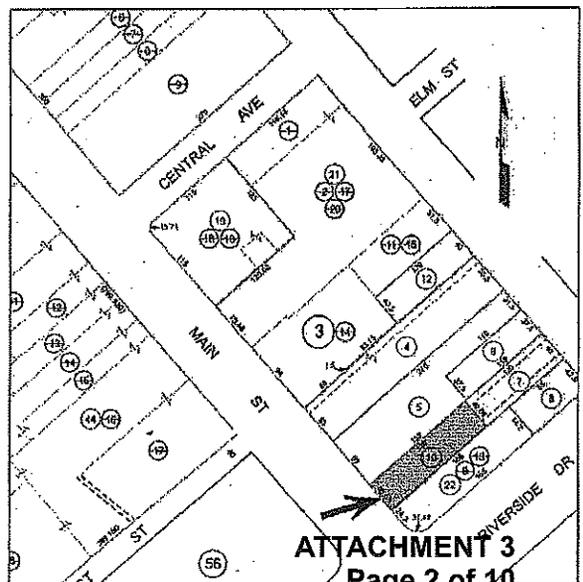
Fred Oda, telephone interview, 8/4/14.

B13. Remarks:

*B14. Evaluator: Anthony Kirk, Ph.D.

*Date of Evaluation: 8/5/14

(This space reserved for official comments.)



ATTACHMENT 3
Page 2 of 10

*Required Information

P3a. Description:

174 follow the walls of the canted entrance, while the slightly larger square windows at 178 are set exclusively in the exterior wall. A fixed aluminum-sash window, slightly above the hotel entrance, illuminates the stairway. Along the second floor, aluminum- and vinyl-sash sliding and one-over-one double-hung windows pierce the north, east, and south sides. The flat roofs of both the one-story and two-story blocks are finished with composition sheets.

As originally constructed in 1926, the Kelly Block was rectangular in plan and rested on a concrete-slab foundation. The ground floor was divided into two rental spaces, with storage at the rear; the upper floor held a small hotel. The entrance doors to the two stores were flanked by display windows above low panels, as would have been typical of the times. A relatively early photograph, taken during the flood of 1938 (figure 3), shows the presence of rolled canvas awnings, used to shade the storefront. Above the doors and windows runs a tall band of prismatic transom windows, some of which may have been casements, allowing for the circulation of air. A sign advertising the hotel projects over the sidewalk. It is unknown if corrugated metal panels formed the wall cladding at the rear, but clearly stucco covered only the front of the building. Fenestration on the upper floor consisted of one-over-one double-hung wood-sash windows. The flat roof was finished with either composition sheets or tar and gravel.

Tracing the evolution of the building's construction history is in large part dependent on the presence of historical photographs and physical evidence. Only four images have been found to date, however, and the interior of the building was not accessible during the site survey. It is evident, nonetheless, that as late as 1976 large display windows flanked the entrances to the two stores on the ground floor and that more than half the band of original transom windows still survived. The entrance to 174 Main Street, at the south end, was at this time recessed more than three feet behind the front of the building envelope, while the entrance to 178 Main Street, at the north end, was flush with the building envelope. Although the hotel had been renovated a half-dozen years earlier, the upper floor was still illuminated by the original one-over-one double-hung wood-sash windows. Corrugated-metal sheet may have covered the rear concrete walls. The flat roof was finished with composition sheets.

In 1987 the storefront was modernized and improved, and it was quite possibly at this time that the large display windows were removed and the tile veneer and smoked windows installed. As part of the project, the door to 178 Main Street was set back from the sidewalk in an entrance canted on the south side, and the section of the façade that formerly held the transom windows was covered with T1-11 plywood siding.

The building at 174-178 Main Street, which appears to be in poor condition, is set at the edge of the lot line to the west, north, and south sides, with an asphalt parking lot to the rear. Most of the surrounding area north of Pajaro River has been redeveloped over the years, including, most recently, the 200-block Watsonville Civic Center between Main and Rodriguez Streets.

B10. Significance:

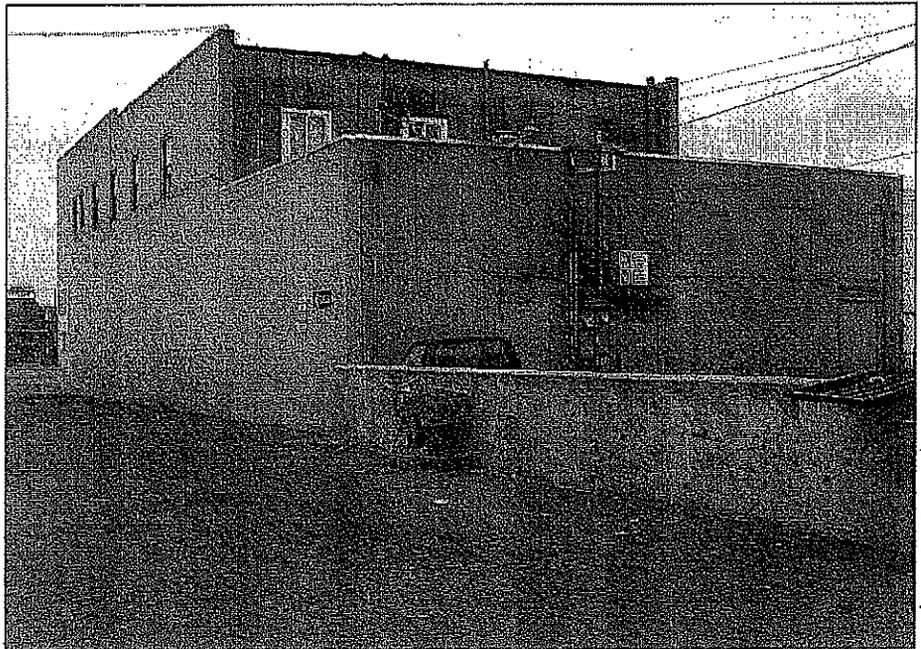
known for its brawls, drug sales, and prostitution and that was overwhelmingly Hispanic. While the Kelly Block originally contributed to the ethnic character of the neighborhood, the storefront has been altered on several occasions over the years, and today it bears no resemblance to its original appearance, the changes having destroyed the architectural features that defined its historic character. The loss of integrity would prevent it from serving as a contributor to a potential historic district illustrating the ethnic heritage of the area, but perhaps even more important than the loss of integrity is the destruction over the years of countless surrounding commercial structures, making it impossible to create a district of associated buildings that would illustrate the Asiatic heritage of the area under Criterion 1.

The building is not associated with an individual known to have made an important contribution to the history of the nation, the state, or the city, as required for Criterion 2. Architecturally, it lacks a cluster of character-defining features associated with a particular style, and apart from the ornamental cornice and shaped parapet, it is devoid of historic detailing. As such it is not significant under Criterion 3. There is, as well, no evidence that it can yield information important to history, as called for by Criterion 4.



Looking northeast at west side,
7/30/14.

Looking northwest at south and east
sides, 7/30/14.



Page 5 of 5

*Resource Name or # (Assigned by recorder) 174-178 Main Street

*Recorded by Anthony Kirk, Ph.D.

*Date 8/5/14

Continuation Update



Kelly Block, 174-178 Main Street,
1938. *Courtesy Pajaro Valley
Historical Association.*

State of California - The Resources Agency DEPARTMENT OF PARKS AND RECREATION PRIMARY RECORD		Primary # HRI #
Other Listings Review Code		Trinomial NRHP Status Code
Reviewer		Date

Page 1 of 5

*Resource Name or #: (Assigned by recorder) 180-186 Main Street

P1. Other Identifier:

*P2. Location: Not for Publication Unrestricted *a. County Santa Cruz

and (P2b and P2c or P2d. Attach a Location Map as necessary)

*b. USGS 7.5' Quad Watsonville West Date 1995 T 12 S; R 2 E; Mount Diablo B.M.

c. Address 180-186 Main Street City Watsonville Zip 95076

d. UTM: (Give more than one for large and/or linear resources) Zone ; mE/ mN

e. Other Locational Data: (e.g., parcel #, directions to resource, elevation, etc., as appropriate)

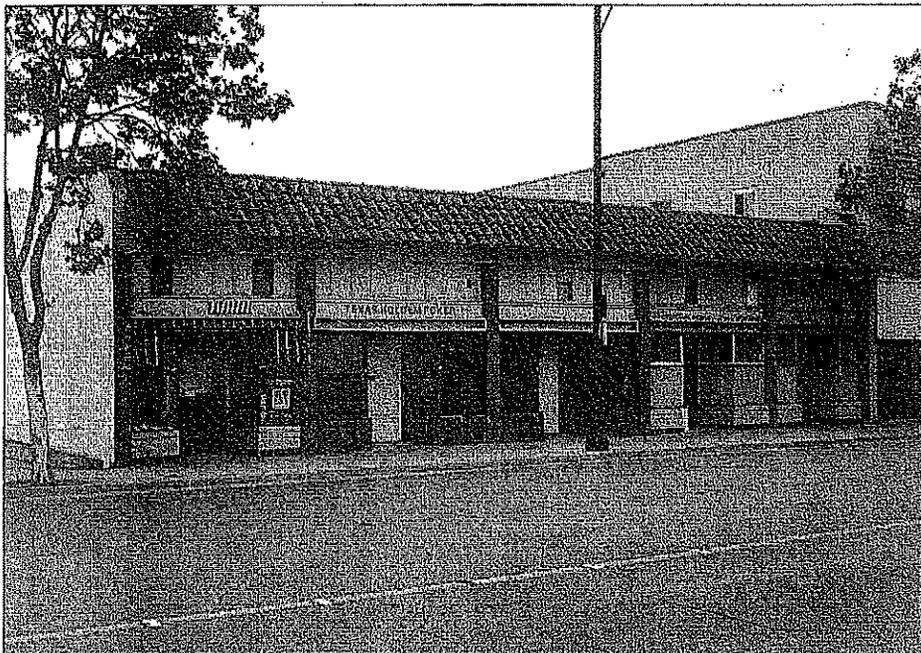
Santa Cruz County APN 017-183-05

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting and boundaries)

The one-story reinforced-concrete Spanish Colonial Revival commercial building at 180-186 Main Street was constructed in 1928 and subsequently altered on several occasions (DPR 523A photo and figures 1 and 2). It is rectangular in plan, encompassing slightly more than 6,000 square feet, and rest on a concrete-slab foundation. The building, which is vacant, faces southwest, or west as it is designated in this report for simplicity, toward Main Street. A shed-roofed hollow-tile storage-room addition projects east from the back of the building. The storefront wall is divided into five separate spaces, each with a canted recessed entrance. With the exception of the former barber shop at the north end, all of the units have been remodeled to varying degrees over the years. In 1950 the dividing wall between the two center units, 182 and 184, was removed, and in the 1970s or 1980s, the two spaces at the south end, 180 and 180½, were merged. Display windows, resting on a tile-clad base, originally rose to the top of the transom window above each glazed door. The two sets of display windows at the south end of the building, 180 Main Street, have been replaced with much smaller aluminum-sash fixed windows above panels with plywood siding and the glazed door replaced with a solid- or hollow-core door, probably sometime in the 1980s. To the north, smoked windows rise from a (continued on page 3)

*P3b. Resource Attributes: (List attributes and codes) HP6 - Commercial Building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)



P5b. Description of Photo: (View, date, accession #) Looking southeast at north and west sides, 7/30/14

*P6. Date Constructed/Age and

Sources: Historic Prehistoric Both

Circa 1926; Commercial Building Record

*P7. Owner and Address:

Fred Oda
39 White Street
Watsonville, CA 95076

*P8. Recorded by: (Name, affiliation, and address)

Anthony Kirk, Ph.D.
1140 East Cliff Drive, No. 3
Santa Cruz, CA 95062

*P9. Date Recorded: 8/5/14

*P10. Survey Type: (Describe)
CEQA Survey

*P11. Report Citation: (cite survey report and other sources, or enter "none.") None

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List)

ATTACHMENT 3
Page 6 of 10

*Required Information

BUILDING, STRUCTURE, AND OBJECT RECORD

*Resource Name or # (Assigned by recorder) 180-186 Main Street

B1. Historic Name: None

B2. Common Name: None

B3. Original Use: Commercial Building

B4. Present Use: Vacant/Not in Use

*B5. Architectural Style: None

*B6. Construction History: (Construction date, alteration, and date of alterations) Constructed 1928; addition to east side ca. 1930s; south end of façade remodeled ca. 1980s; middle section of façade remodeled ca. 1990-2004.

*B7. Moved? No Yes Unknown Date:

Original Location:

*B8. Related Features: None

B9a. Architect: Unknown

b. Builder: Unknown

*B10. Significance: Theme n/a

Area n/a

Period of Significance n/a

Property Type n/a

Applicable Criteria n/a

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Address Integrity.)

The commercial building at 180-186 Main Street does not appear to be eligible for the California Register of Historical Resources. The property is part of the former Chinatown that in the 1920s stretched north along Union, Main, and Rodriguez Streets from the Pajaro River to roughly 1st Street. Despite the name, the area was characterized by a scattering of Chinese establishments and far more Japanese businesses, particularly south of Bridge Street (now Highway 129). Close to the river were lodging houses and residences occupied by a sprinkling of Filipinos and many Chinese and Japanese. In 1938, a decade after commercial building at 180-186 Main Street was completed, the five businesses located in it were Ying Lee Laundry, Utsunomiya Restaurant, Kojini Matsuji Shoe Repair, Ceasar Locatelli Liquors, and Tokuzo Oda, a barber who had maintained a shop in this location since at least 1920 and who acquired the building in this decade. With the bombing of Pearl Harbor, the Japanese were forcibly moved to relocation camps and the area slowly became more Hispanic, as Mexican laborers moved north to work in the fields, taking the positions formerly held by the interred Japanese. By 1950 the building, which had one space vacant, was occupied by Donald Reyes Restaurant, Ceasar's Tavern, which had expanded into two rental units, and the Tokuzo Oda. Over the following decades this section of town became increasingly Hispanic, and by 1971 the two tenants, in addition to the barber-owner, were La Poblinita Café and Ceasar's Tavern. The neighborhood had by this time been "blighted" for some years and was known for the "extremely low rents." Although Oda continued to cut hair, the neighborhood—which (continued on page 3)

B11. Additional Resource Attributes (List attributes and codes):

*B12. References:

B13. Remarks:

Commercial Building Record, 180-186 Main Street, Santa Cruz County Assessor.

Watsonville City Directories (1927-89).

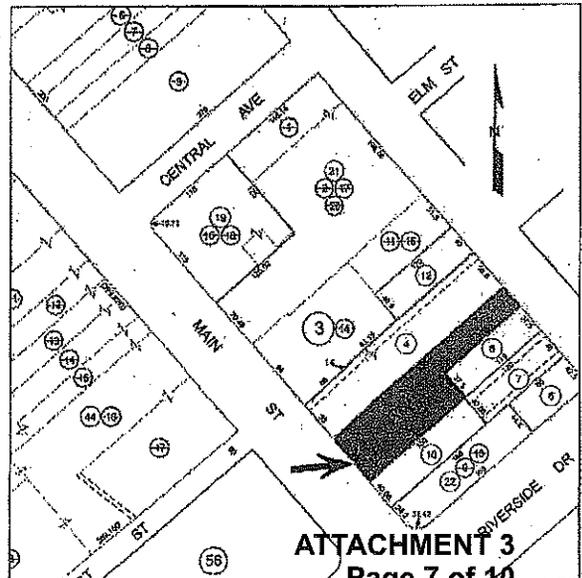
Sanborn Map Co., *Watsonville, California* (1911, 1920, 1939, 1962).

Fred Oda, telephone interview, 8/4/14.

*B14. Evaluator: Anthony Kirk, Ph.D.

*Date of Evaluation: 8/5/14

(This space reserved for official comments.)



ATTACHMENT 3
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*Required Information

P3a. Description:

modern tile-clad base and flank the wood doors of the two units forming 184 Main Street, the remodel probably dating to the 1990s or the early years of this century. At the far north end of the building, 186 Main Street, the original display windows, held by thin aluminum sash, extend from the tile-clad base to the top of the transom window above the original glazed door. Above the windows, awnings were furled or extended over the storefronts; today only a single awning survives, at 186 Main Street. A row of transom windows, two of which are casements, extends along the top of fascia boards above each unit. A stucco-clad wall rises to a shed roof that, extending out from the top of the parapet, rests on decoratively carved rafters and is finished with mission tiles. At the rear of the building, a row of five four-light swivel windows runs along the east wall beneath a row of large ten-light industrial windows. All the windows are missing panes or have been damaged and patched in various ways. The flat roof is finished with tar and gravel and holds five large skylights.

The commercial building at 180-186 Main Street, which appears to be in poor condition, is set at the edge of the parcel line on the west, north, and south sides. Situated behind the building is a small yard filled with weeds and trash and a one-story wood-frame single-family residence, with an integral one-car garage, that was built in 1965 (figure 3). A hipped-roof concrete entry porch is located on the south side of the house. The south and east sides are clad with rabbeted bevel siding, the north and west sides with stucco. Fenestration is asymmetrical and consists of sliding aluminum-sash windows, except for a large combination window in the living room. The side-gabled roof is characterized by open eaves of moderate overhang and is finished with composition shingles. An overhead swing-up door provides access to the garage, which is entered from the east end of the lot on Union Street. Most of the surrounding area north of the Pajaro River has been redeveloped over the years, including, most recently, the 200-block Watsonville Civic Center between Main and Rodriguez Streets.

B10. Significance:

increasingly became known for its brawls and drug sales, drinking and prostitution—was overwhelmingly Hispanic. While the commercial block originally contributed to the original ethnic character of the neighborhood, the storefront has been altered on several occasions over the years and has suffered the loss of some architectural features that defined its historic character. Although it is debatable whether or not the structure maintains integrity, the destruction over the years of countless surrounding building that made up the neighborhood has made it impossible to create a district of associated buildings that would illustrate the theme of Asiatic heritage of the area under Criterion 1.

The building is not associated with an individual known to have made an important contribution to the history of the nation, the state, or the city, as required for Criterion 2. Architecturally, it is an extremely simple example of Spanish Colonial Revival architecture, possessing a single feature of the style, the pitched tiled roof, and as such it is not significant under Criterion 3. There is, as well, no evidence that it can yield information important to history, as called for by Criterion 4. The house at the rear of the commercial building, at 186 Main Street, is less than fifty years old, having been completed in 1965. The building is clearly not of exceptional importance and does not meet Criteria Consideration G for properties that have achieved significance in the last fifty years.



Figure 1. Looking southeast at west side, 7/30/14.

Figure 2. Looking northwest at east side, 7/30/14.

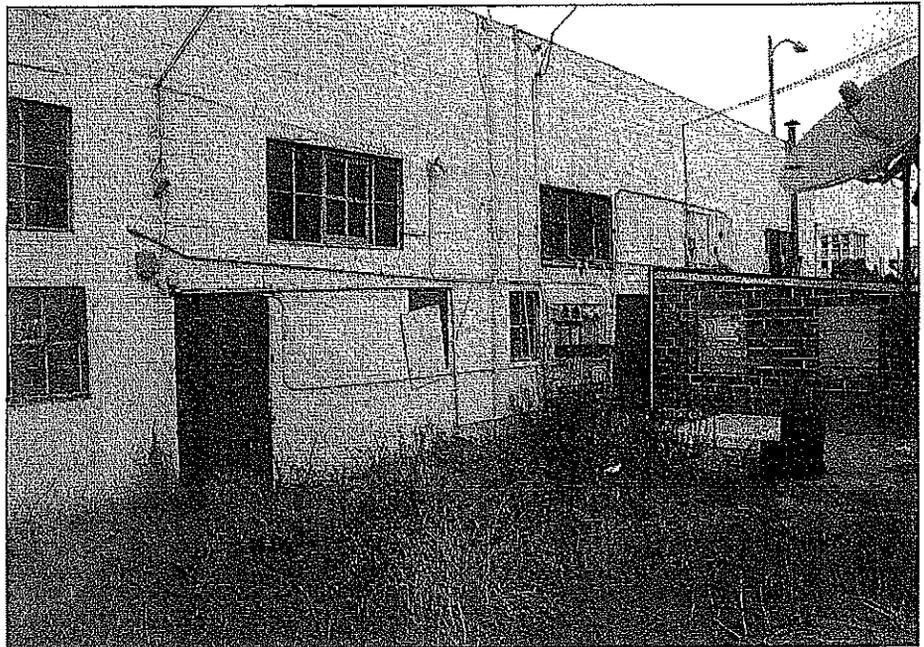
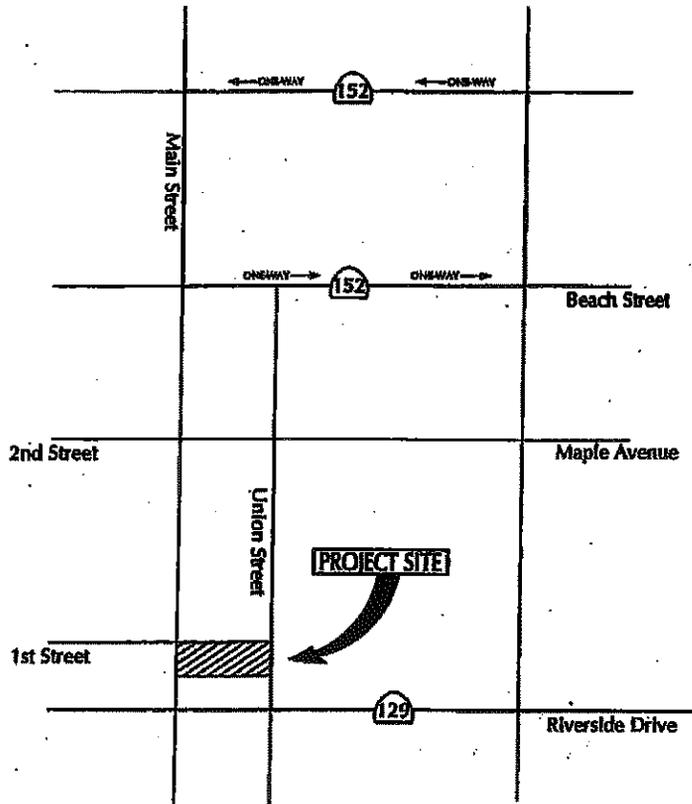




Figure 3. House on east side of commercial building, constructed 1965.. Looking northeast at west and south sides, 7/30/14.

**MCDONALD'S
WATSONVILLE, CALIFORNIA**

TRAFFIC AND CIRCULATION STUDY



February 5, 2014

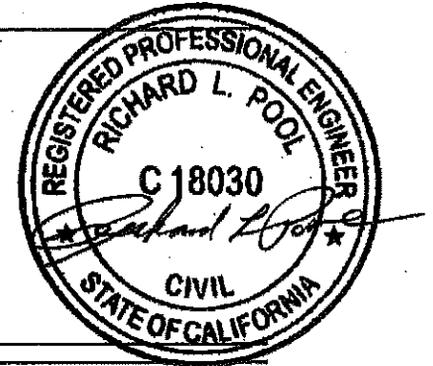
ATE Project 13036

Prepared for:

Prepared by:

McDonald's USA, LLC
2999 Oak Road, Suite 900
Walnut Creek, CA 94597

Darryl F. Nelson, PTP
Under the direction of:
Richard L. Pool, P.E.



ASSOCIATED TRANSPORTATION ENGINEERS

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ATTACHMENT 4
Page 1 of 20



ASSOCIATED TRANSPORTATION ENGINEERS

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Since 1878

Richard L. Pool, P.E.
Scott A. Schell, AICP, PTP

February 5, 2014

Ms. Margaret Trujillo
Area Construction Manager
Pacific Sierra Region
McDonald's USA LLC
2999 Oak Road, Suite 900
Walnut Creek, California 94597

TRAFFIC AND CIRCULATION STUDY FOR THE McDONALD'S - CITY OF WATSONVILLE

Associated Transportation Engineers (ATE) is pleased to submit the following traffic and circulation study for the McDonald's. The study examines existing and future conditions within the study-area and assesses the traffic and circulation impacts associated with the project. It our understanding that the results of the traffic and circulation study will be used by the City of Watsonville to process the project's development application.

We appreciate the opportunity to assist McDonald's USA, LLC with this project.

Associated Transportation Engineers

By: Richard L. Pool, P.E.
President



ATTACHMENT 4

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INTRODUCTION

This traffic study contains an analysis of the potential traffic and circulation impacts associated with the proposed McDonald's in the southeastern portion of the City of Watsonville. The study provides information relative to existing, existing + project, cumulative (existing + approved/pending projects) and cumulative + project peak period conditions. Site access and circulation are also addressed in the traffic study.

PROJECT DESCRIPTION

The McDonald's is located on the east side of Main Street, north of Riverside Drive (State Route 129) in the City of Watsonville, as shown on Figure 1. The project is proposing to redevelop a site currently occupied by two commercial buildings and a single family home with a 4,356 square foot McDonald's with a dual order board drive-through lane. Access to the project site will be provided by driveway connections on Main Street and Union Street. The project site plan is illustrated on Figure 2.

EXISTING CONDITIONS

Existing Street Network

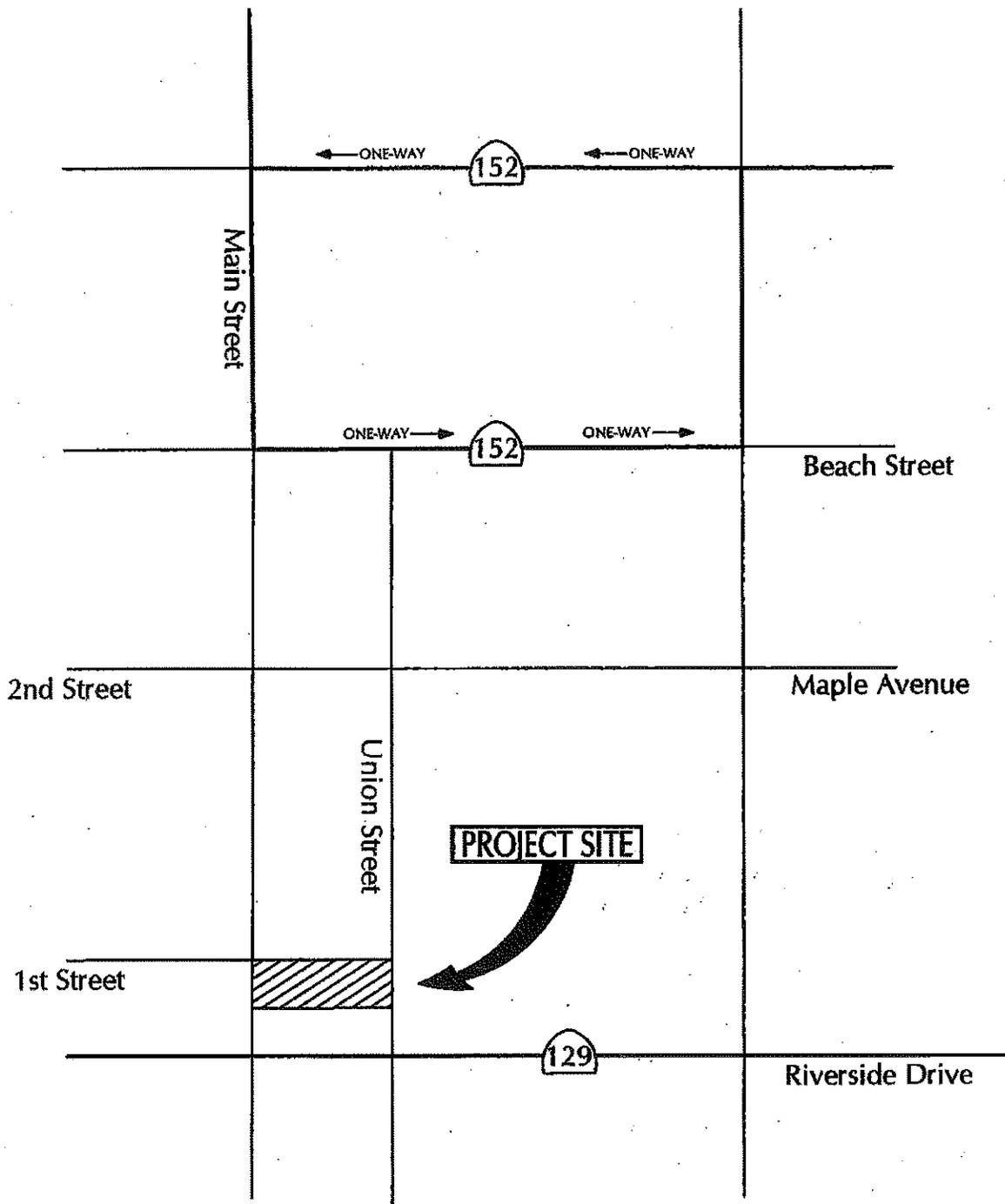
The project site is served by a circulation system comprising of arterials and collector streets, which are illustrated on Figure 1. The major roadways serving the site are discussed in the following text.

Riverside Drive (State Route 129), located south the project site, is a east-west arterial street, extends easterly from State Route 1 through the City of Watsonville. From State Route 1 to Salsipuedes Creek, Riverside Drive is a 4-lane divided arterial roadway. Riverside Drive is a 2-lane undivided roadway east of Salsipuedes Creek. The Main Street/Riverside Drive and Union Street/Riverside Drive intersections are signalized.

Beach Street (State Route 152), located north of the project site, is a east-west arterial street, that extends easterly from Rio Boca Road to Hushbeck Avenue. Through the study-area Beach Street is a 2-lane one-way couplet until it reaches Lincoln Street. The Main Street/Beach Street intersection is signalized.

Main Street, is a north-south arterial roadway located adjacent to the project site, that extends northerly from San Juan Road to the western portion of the City of Watsonville where it merges with State Route 1. South of San Juan Road, Main Street becomes Porter Drive. Through the study-area Main Street is a 4-lane arterial roadway with a center turn lane. Main Street provides access to the project site. In the study-area, this roadway is signalized at Riverside Drive and Beach Street.

Union Street, is 2-lane north-south collector roadway from Front Street to Brennan Street. Union Street provides access to the project site. Within the study-area, Union Street is signalized at Riverside Drive.



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EXISTING STREET NETWORK/PROJECT SITE LOCATION

FIGURE 1

ATTACHMENT 4
Page 6 of 20

First Street, is a 2-lane local roadway that extends easterly from Riverside Drive to Main Street. The Main Street/First Street intersection is STOP-sign controlled on First Street. During the A.M. and P.M. peak hour periods, left-turns to and from First Street are restricted. The project driveway is located opposite First Street.

Existing Volumes and Levels of Service

Intersection Operations

Traffic flow on urban arterials is most constrained at intersections. Therefore, a detailed analysis of traffic flows must examine the operating conditions of critical intersections during peak travel periods. In rating the intersection operations, "Levels of Service" (LOS) "A" through "F" are used, with LOS "A" indicating free flow operations and LOS "F" indicating congested operations (more complete definitions of levels of service are included in the Technical Appendix).

Figure 3 illustrates the four study-area intersections, the existing traffic controls and the intersection geometries. Existing A.M. and P.M. peak hour period traffic volumes at the study-area intersections are shown on Figure 4. These volumes were collected in September of 2013 by Associated Transportation Engineers (ATE). Existing levels of service for the study-area intersections were calculated using the Highway Capacity Manual methodologies as required by the City of Watsonville. In the City of Watsonville LOS "D" or better is acceptable for intersection operation. Worksheets illustrating the level of service calculations are contained in the Technical Appendix for reference. Table 1 lists the level of service for the nine study-area intersections during the A.M. and P.M. peak hour periods.

**Table 1
Existing Peak Hour Levels of Service**

Intersection	Control Type	A.M. Peak Hour		P.M. Peak Hour	
		Delay	LOS	Delay	LOS
Main Street/Beach Street	Signal	13.7 sec.	LOS B	18.8 sec.	LOS B
Main Street/First Street	STOP-Sign	0.3 sec.	LOS A	0.7 sec.	LOS A
Main Street/Riverside Drive	Signal	23.3 sec.	LOS C	41.9 sec.	LOS D
Union Street/Riverside Drive	Signal	18.0 sec.	LOS B	20.3 sec.	LOS C

The four study-area intersections currently operate at LOS "D" or better during the A.M. peak hour and P.M. peak hour periods.

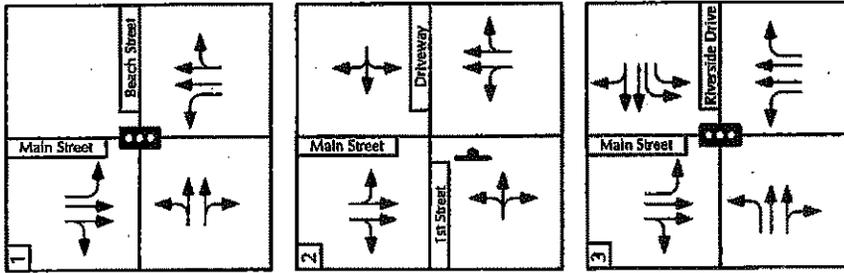
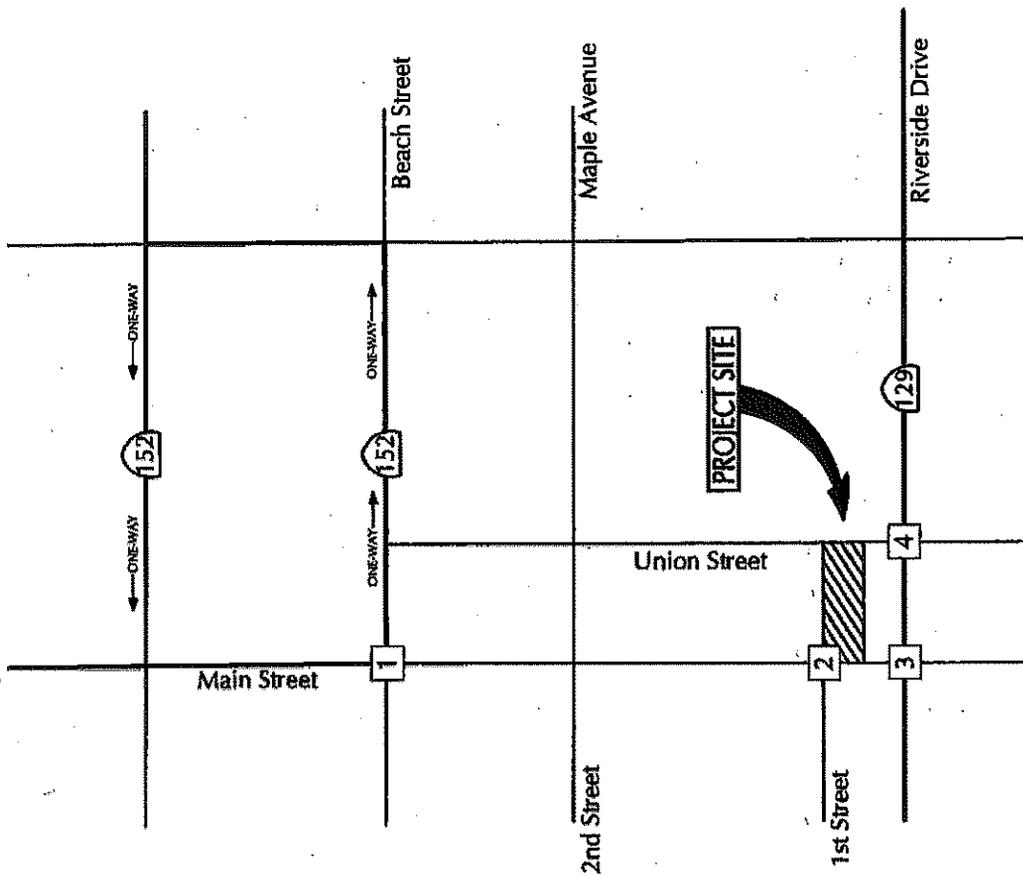


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EXISTING LANE GEOMETRY AND TRAFFIC CONTROL

FIGURE 3

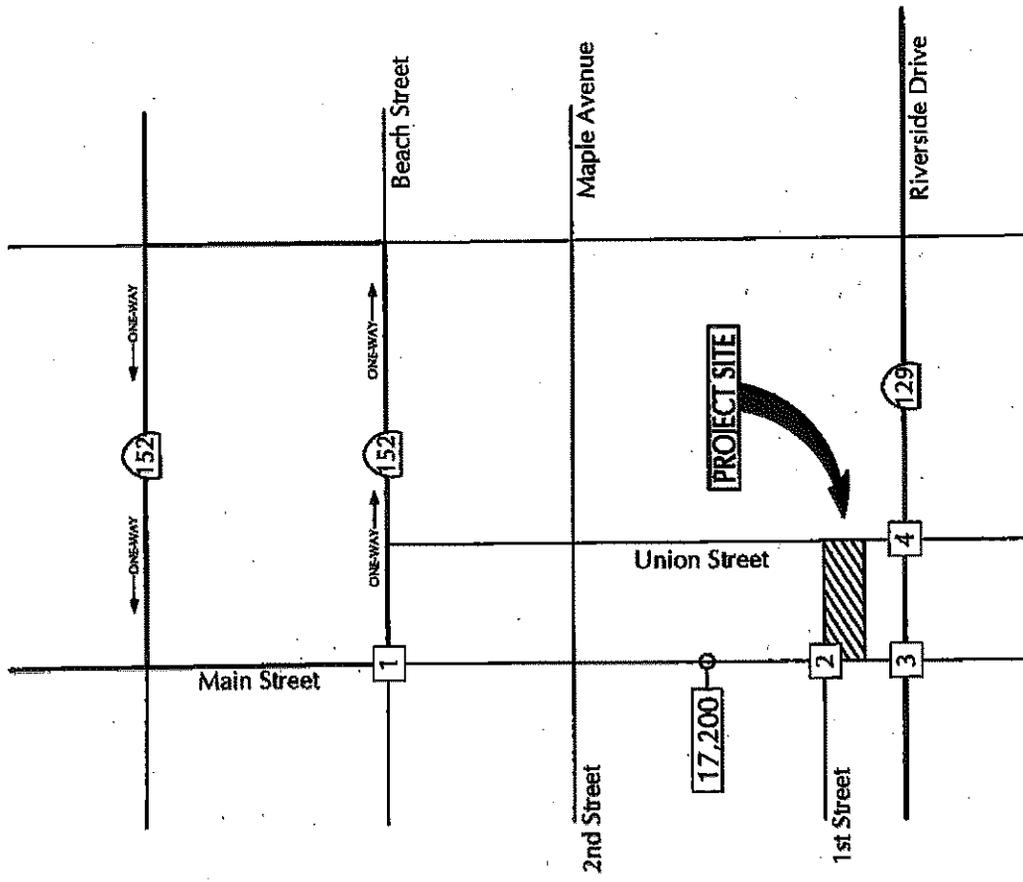
MMWF - #13036



LEGEND

- Lane Geometry
- Signalized Intersection
- Stopped Approach





<table border="1"> <tr> <td>269(122)</td> <td>57(39)</td> <td>(101)176</td> </tr> <tr> <td>722(465)</td> <td>381(228)</td> <td>(431)490</td> </tr> <tr> <td>43(63)</td> <td>58(23)</td> <td>(40)44</td> </tr> </table>	269(122)	57(39)	(101)176	722(465)	381(228)	(431)490	43(63)	58(23)	(40)44	<table border="1"> <tr> <td>5(2)</td> <td>(0)2</td> </tr> <tr> <td>626(355)</td> <td>(12)10</td> </tr> <tr> <td>72(75)</td> <td>(615)789</td> </tr> <tr> <td>0(0)</td> <td>(0)0</td> </tr> <tr> <td>2(1)</td> <td></td> </tr> <tr> <td>77(34)</td> <td></td> </tr> </table>	5(2)	(0)2	626(355)	(12)10	72(75)	(615)789	0(0)	(0)0	2(1)		77(34)		<table border="1"> <tr> <td>125(50)</td> <td>5(4)93</td> <td>(292)293</td> </tr> <tr> <td>514(323)</td> <td>(470)426</td> <td>(493)605</td> </tr> <tr> <td>39(19)</td> <td>(236)367</td> <td>(353)333</td> </tr> <tr> <td>105(68)</td> <td></td> <td></td> </tr> <tr> <td>536(341)</td> <td></td> <td></td> </tr> <tr> <td>466(242)</td> <td></td> <td></td> </tr> </table>	125(50)	5(4)93	(292)293	514(323)	(470)426	(493)605	39(19)	(236)367	(353)333	105(68)			536(341)			466(242)		
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34(20)																																									

LEGEND

X - Average Daily Traffic Volume

XX - (A.M.) P.M. Peak Hour Volume

NOT TO SCALE

FIGURE 4

EXISTING TRAFFIC VOLUMES

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ENGINEERS



MMF - #13036

PROJECT-SPECIFIC ANALYSIS

Project Trip Generation

The project is proposing to redevelop a site currently occupied by two commercial buildings and a single family home with a 4,356 square foot McDonald's with a tandem drive-through. For the purposes of estimating the number of trips which would be generated by the 4,365 square foot McDonald's restaurant, rates for "Fast-Food Restaurant with Drive-Through Window (Land Use Code #934) published in the Institute of Transportation Engineering (ITE), Trip Generation, 9th Edition were used. Table 2 summarizes the average daily, A.M. and P.M. peak hour trip generation estimated for the McDonald's. The trip generation estimates assumes a 50% "pass-by" trip factor. "Pass-by" trips are not new trips, but trips which already exist on the adjacent street system. The "pass-by" rate was developed from ITE data for "Fast-Food Restaurants with Drive-Through Windows".

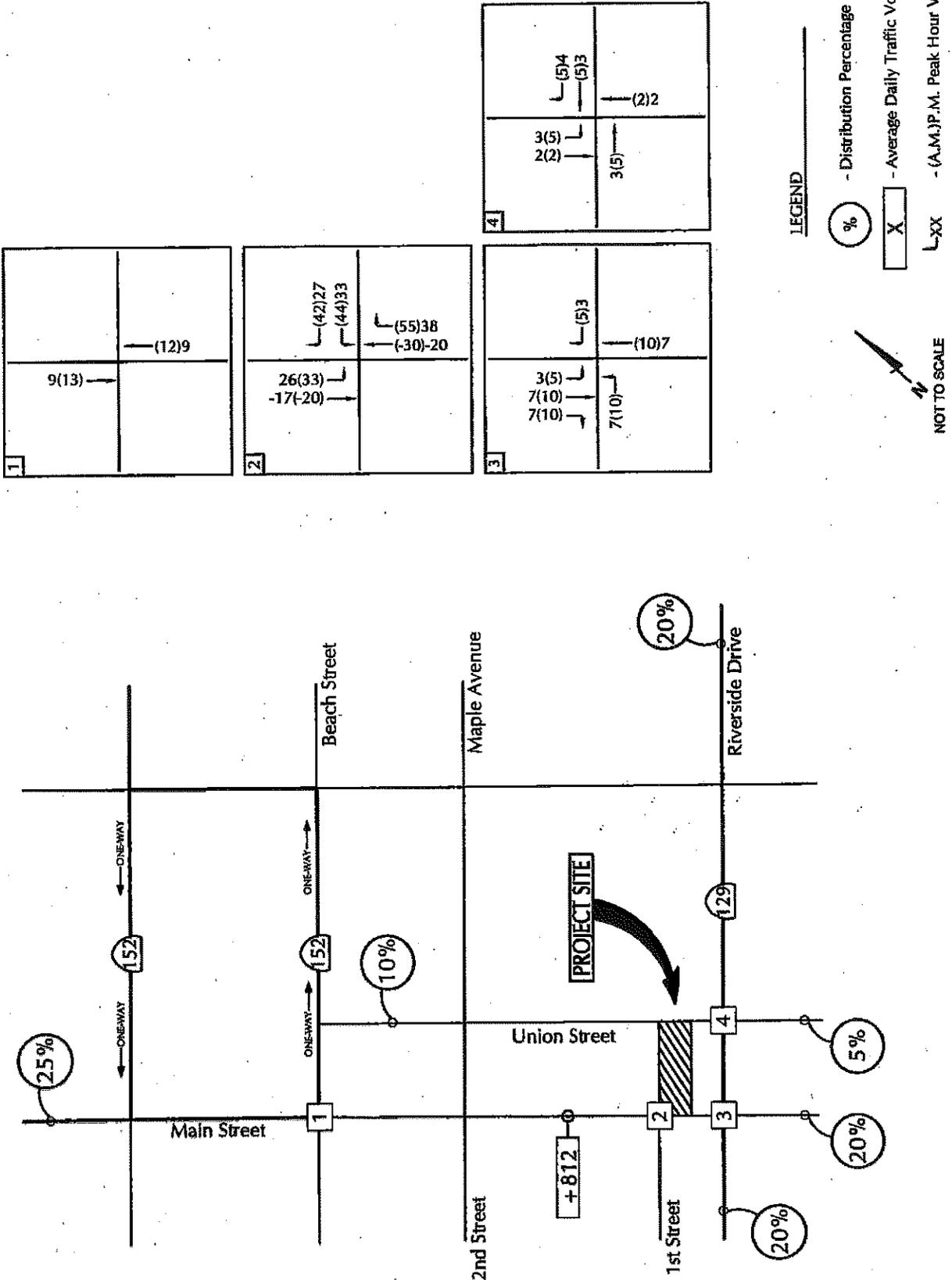
**Table 2
Project Trip Generation**

Land Use	Size	ADT	Weekday Peak Hour Trips					
			A.M. Peak Hour			P.M. Peak Hour		
			Entering	Exiting	Total	Entering	Exiting	Total
Fast-Food Restaurant with Drive - Thru	4,365 sq.ft.	2,166	100	98	198	74	68	142
Less 50% Pass-By Trips:		-1,083	-50	-49	-99	-37	-34	-71
Total Primary Trips:		1,083	50	49	99	37	34	71

As shown in Table 2 the project residential land-uses would generate a total of 2,166 average daily trips, 198 A.M. peak hour trips and 142 P.M. peak hour trips. A total of 1,083 average daily trips, 99 A.M. peak hour trips and 71 P.M. peak hour trips would be "pass-by" trips. The remaining 1,083 average daily trips, 99 A.M. peak hour trips and 71 P.M. peak hour trips would be new primary trips added to the adjacent street system.

Project Trip Distribution and Assignment

The project-generated A.M. and P.M. peak hour traffic volumes were distributed and assigned to the study-area intersections based on travel data derived from the existing traffic volumes as well as a general knowledge of the population employment and commercial centers in the Watsonville area. Figure 5 illustrates the trip distribution and assignment assumed for the project's trips.



Existing + Project Intersection Operations

Levels of service were calculated for the study-area intersections assuming the Existing + Project traffic volumes illustrated on Figure 6. Tables 3 and 4 compare to the Existing and Existing + Project levels of service and identify the potential project-specific impacts.

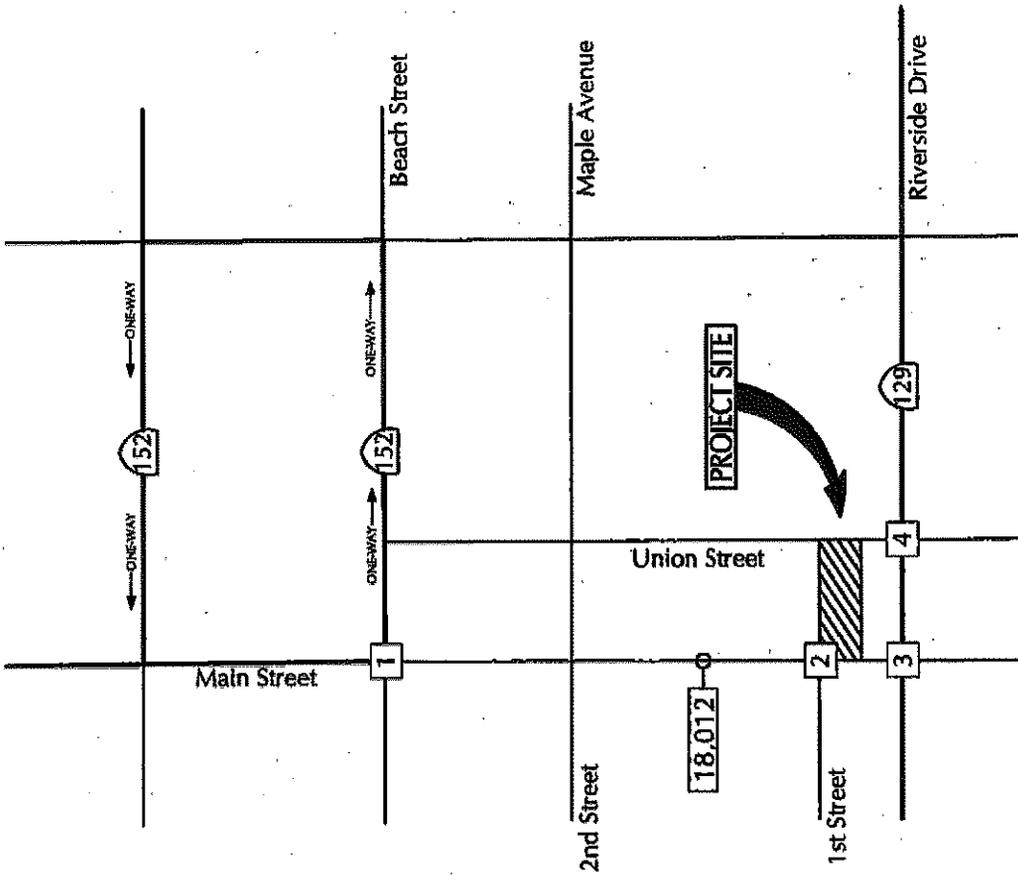
**Table 3
Existing + Project A.M. Peak Hour Levels of Service**

Intersection	Existing		Existing + Project		
	Delay	LOS	Delay	LOS	Impact
Main Street/Beach Street	13.7 sec.	LOS B	13.6 sec.	LOS B	No
Main Street/First Street	0.3 sec.	LOS A	2.0 sec.	LOS A	No
Main Street/Riverside Drive	23.3 sec.	LOS C	23.5 sec.	LOS C	No
Union Street/Riverside Drive	18.0 sec.	LOS B	17.7 sec.	LOS B	No

**Table 4
Existing + Project P.M. Peak Hour Levels of Service**

Intersection	Existing		Existing + Project		
	Delay	LOS	Delay	LOS	Impact
Main Street/Beach Street	18.8 sec.	LOS B	18.9 sec.	LOS B	No
Main Street/First Street	0.7 sec.	LOS A	2.4 sec.	LOS A	No
Main Street/Riverside Drive	41.9 sec.	LOS D	40.9 sec.	LOS D	No
Union Street/Riverside Drive	20.3 sec.	LOS C	18.7 sec.	LOS B	No

The data presented in Tables 3 and 4 indicate that the project would not have a significant impact to the study-area intersections. The study-area intersections would continue to operate at LOS D or better during the A.M. or the P.M. peak hour periods.



1	269(122) 731(478) 43(63)	57(39) 381(228) 58(23)	(101)176 (443)499 (40)44
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2	26(33) 609(335) 72(75)	0(0) 2(1) 77(34)	(42)27 (44)33 (55)38 (585)769 (0)0
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3	128(55) 521(333) 46(29)	112(78) 536(341) 466(242)	(59)96 (470)426 (236)367 (292)293 (503)612 (353)333
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4	38(17) 36(11) 132(91)	95(64) 829(603) 34(20)	(30)63 (648)678 (8)21 (6)22 (22)55 (40)50
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LEGEND

X - Average Daily Traffic Volume

XX - (A.M.) P.M. Peak Hour Volume

NOT TO SCALE

FIGURE 6

EXISTING + PROJECT TRAFFIC VOLUMES

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TRANSPORTATION
ENGINEERS



CUMULATIVE CONDITIONS

The City of Watsonville requires that the intersections be analyzed with the addition of traffic generated by projects which have been approved or are pending within the project study-area. Trip generation estimates were developed for the cumulative development projects using rates presented in the ITE, Trip Generation, 9th Edition. Table 5 summarizes the average daily, A.M. and P.M. peak hour trip generation for the approved/pending projects.

**Table 5
Approved/Pending Development Projects Trip Generation**

No.	Project	Land Use	Size	ADT	A.M. Peak Hour	P.M. Peak Hour
1.	355 Harvest Drive	Light Industrial	15,000 SF	104	14	14
2.	1 Western Drive	Hotel	48 Rooms	428	32	34
3.	260 Riverside	Warehouse	27,776 SF	99	8	9
4.	1820 Main Street	Medical Clinic	3,310 SF	120	8	12
5.	398 Technology Drive	Medical Office	40,000 SF	1,445	96	143
6.	1000 Main Street	Grocery Store	15,000 SF	1,534	51	142
Total Trips:				3,730	209	354

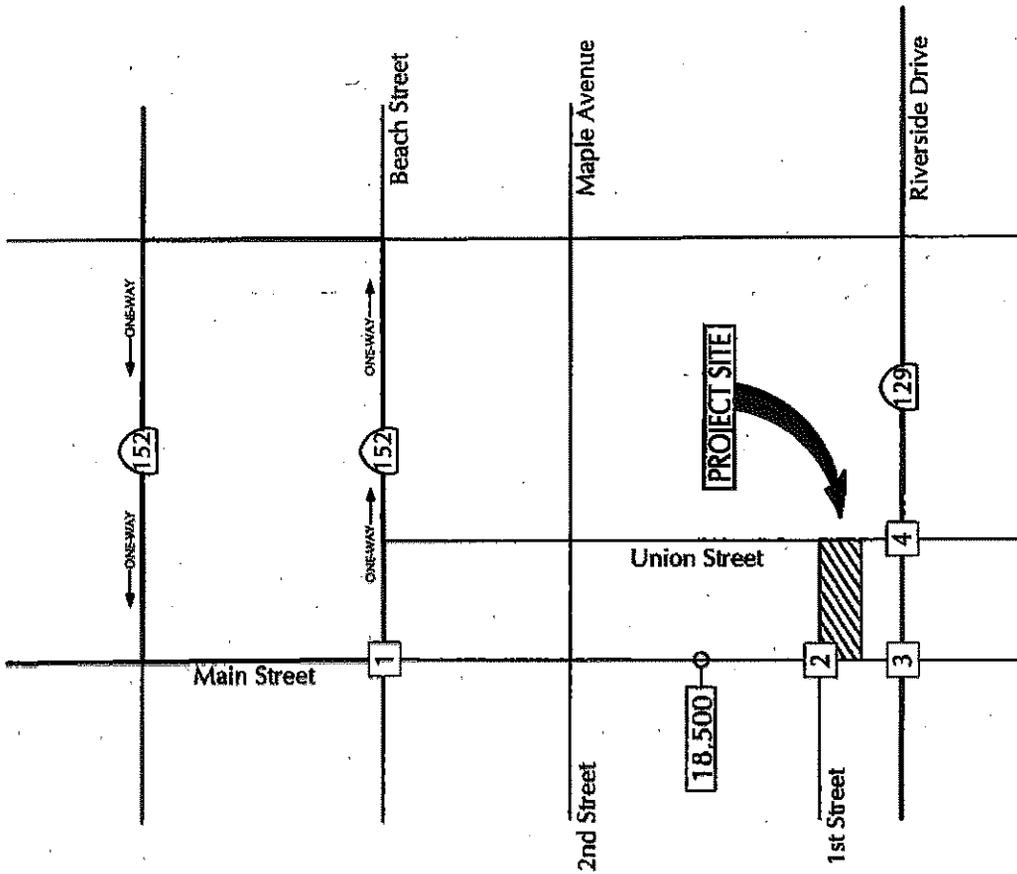
The data presented in Table 5 indicates that the approved/pending projects would generate a total of 3,730 average daily trips, 209 A.M. peak hour trips and 354 P.M. peak hour trips. The approved/pending projects' peak hour traffic volumes were distributed and assigned to the study-area intersections. The trip assignment for the cumulative development projects was developed based on the location of each project, recent traffic studies, existing traffic patterns observed in the study area as well as a general knowledge of the population, employment and commercial centers in Watsonville and the surrounding area. Figure 7 illustrates the Cumulative traffic volumes at the study-area intersections.

Cumulative + Project Intersection Operations

Levels of service were calculated for the study-area intersections assuming the Cumulative + Project traffic volumes illustrated on Figure 8. Cumulative and Cumulative + Project levels of service for the four study-area intersections are compared in Tables 6 and 7.

**Table 6
Cumulative + Project A.M. Peak Hour Levels of Service**

Intersection	Cumulative		Cumulative + Project		
	Delay	LOS	Delay	LOS	Impact
Main Street/Beach Street	13.9 sec.	LOS B	13.7 sec.	LOS B	No
Main Street/First Street	0.3 sec.	LOS A	2.1 sec.	LOS A	No
Main Street/Riverside Drive	23.5 sec.	LOS C	23.9 sec.	LOS C	No
Union Street/Riverside Drive	17.8 sec.	LOS B	18.2 sec.	LOS B	No



1

269(122) 798(486) 43(63)	57(39) 381(228) 58(23)	(101)176 (483)538 (40)44
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2

5(2) 702(376) 72(75)	0(0) 2(1) 77(34)	(0)2 (12)10 (667)837 (0)0
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3

140(55) 560(334) 54(24)	115(78) 536(341) 466(242)	(64)103 (470)426 (236)367 (292)293 (525)633 (353)333
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4

35(12) 34(9) 132(91)	95(64) 841(603) 34(20)	(25)59 (653)665 (8)21 (6)22 (20)53 (40)50
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LEGEND

X - Average Daily Traffic Volume

L-XX - (A.M.) P.M. Peak Hour Volume

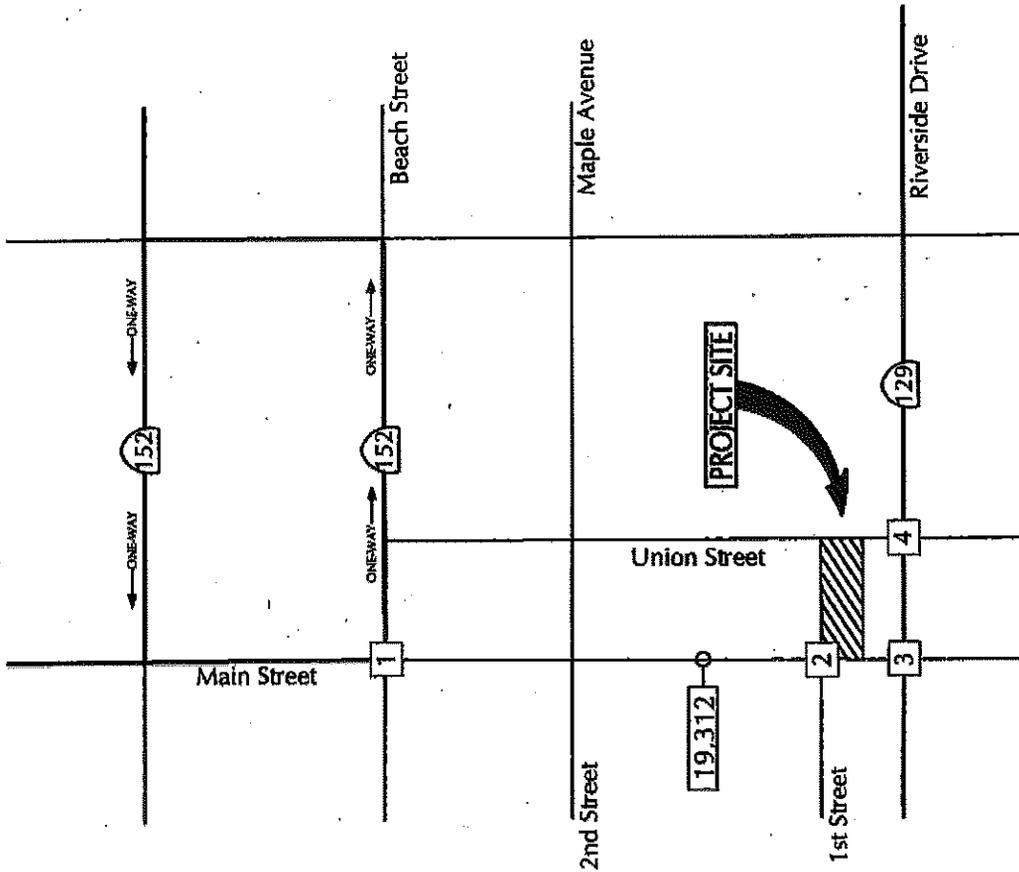
NOT TO SCALE

FIGURE 7

CUMULATIVE TRAFFIC VOLUMES

ASSOCIATED
TRANSPORTATION
ENGINEERS





<p>1</p> <table border="1"> <tr> <td>269(122) 807(499) 43(63)</td> <td>57(39) 381(228) 58(23)</td> <td>(101)176 (495)547 (40)44</td> </tr> </table>	269(122) 807(499) 43(63)	57(39) 381(228) 58(23)	(101)176 (495)547 (40)44	<p>2</p> <table border="1"> <tr> <td>26(33) 685(356) 72(75)</td> <td>0(0) 2(1) 77(34)</td> <td>(42)27 (44)33 (55)38 (637)817 (0)0</td> </tr> </table>	26(33) 685(356) 72(75)	0(0) 2(1) 77(34)	(42)27 (44)33 (55)38 (637)817 (0)0	<p>3</p> <table border="1"> <tr> <td>143(60) 567(344) 61(34)</td> <td>122(88) 536(341) 466(242)</td> <td>(69)106 (470)426 (236)367 (292)293 (535)640 (353)333</td> </tr> </table>	143(60) 567(344) 61(34)	122(88) 536(341) 466(242)	(69)106 (470)426 (236)367 (292)293 (535)640 (353)333
269(122) 807(499) 43(63)	57(39) 381(228) 58(23)	(101)176 (495)547 (40)44									
26(33) 685(356) 72(75)	0(0) 2(1) 77(34)	(42)27 (44)33 (55)38 (637)817 (0)0									
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<p>4</p> <table border="1"> <tr> <td>38(17) 36(11) 132(91)</td> <td>95(64) 844(508) 34(20)</td> <td>(30)63 (658)688 (6)21 (6)22 (22)55 (40)50</td> </tr> </table>	38(17) 36(11) 132(91)	95(64) 844(508) 34(20)	(30)63 (658)688 (6)21 (6)22 (22)55 (40)50								
38(17) 36(11) 132(91)	95(64) 844(508) 34(20)	(30)63 (658)688 (6)21 (6)22 (22)55 (40)50									

LEGEND

X - Average Daily Traffic Volume

XX - (A.M.)P.M. Peak Hour Volume

NOT TO SCALE

FIGURE 8

CUMULATIVE + PROJECT TRAFFIC VOLUMES

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**Table 7
Cumulative + Project P.M. Peak Hour Levels of Service**

Intersection	Cumulative		Cumulative + Project		
	Delay	LOS	Delay	LOS	Impact
Main Street/Beach Street	19.1 sec.	LOS B	19.1 sec.	LOS B	No
Main Street/First Street	0.7 sec.	LOS A	2.7 sec.	LOS A	No
Main Street/Riverside Drive	42.7 sec.	LOS D	44.7 sec.	LOS D	No
Union Street/Riverside Drive	18.5 sec.	LOS B	18.7 sec.	LOS B	No

The data presented in Tables 6 and 7 indicate that the project would not have a significant impact to the study-area intersections. The study-area intersections would continue to operate at LOS D or better during the A.M. or the P.M. peak hour periods.

SITE ACCESS AND CIRCULATION

As shown on Figure 2, access to the McDonald's will be provided by driveway connections on Main Street and Union Street. Inbound and outbound only driveways will be provided on both Main Street and Union Street. The Main Street access will be opposite First Street. During the A.M. and P.M. peak hour periods, left-turns to and from First Street are restricted, however full access to and from the project will be allowed at all times. The site access driveways will be designed according to City of Watsonville design standards. Given the existing and forecasted traffic volumes, the project access driveways on Main Street and Union Street should operate acceptably and accommodate project traffic volumes.

The proposed dual order board drive through and parking configuration would allow vehicles to enter the drive through lanes from Main Street or Union Street. Vehicles entering from Main Street would circulate counter-clock wise around the building then turn left to enter the drive through. Vehicles entering from Union Street would just need to veer left to enter the drive through. Drivers leaving the drive through would to exit onto Main Street via the out only driveway.

Drive-Through Queue Analysis

Drive trough vehicle queue data was collected by ATE at several McDonald's to determine if the storage area provided at the proposed Watsonville McDonald's would be sufficient. The sites included a McDonald's in Fountain Valley on Brookhurst Street, a Goleta McDonald's on Fairview Avenue, a Santa Maria site located within a Costco shopping center, and a site in Canoga Park located within a shopping center development. Table 8 summarizes the queue data.

**Table 8
McDonald's Restaurant Queue Study Results**

Restaurant Location	Vehicles in Queue	
	Average	Maximum
Fountain Valley	2 Vehicles	5 Vehicles
Goleta	6 Vehicles	12 Vehicles
Santa Maria	3 Vehicles	9 Vehicles
Canoga Park	6 Vehicles	10 Vehicles
Average	4 Vehicles	9 Vehicles

The data presented in Table 8 indicate that the peak queue observed at the existing Fountain Valley McDonald's was 5 vehicles, 12 vehicles at the Goleta site, while the peak queues observed at the Santa Maria and Canoga Park sites were 9 and 10 vehicles, respectively. Review of the ATE field data show the maximum 12-vehicle queue in Goleta occurred at 12:15 P.M. and lasted for 1 minute and 30 seconds. The average queues observed at the four sites ranged from 2 to 6 vehicles. The measured vehicle queue is from the pick-up window.

Based on the study data reviewed above and the site design proposed for the proposed Watsonville McDonald's, it is concluded that the 12-vehicle storage area proposed for the dual order board drive-through lane would provide adequate space to accommodate the queues observed at the various site during the majority of the peak periods. Should a queue of 12 vehicles be experienced at the Watsonville site (as occurred at the Goleta site), the 12th car in line would not block parking spaces in the parking area located closest to the drive-through lane entrance or the travel lane. It is however anticipated that 12 car vehicle queue would be an infrequent occurrence given the size of the proposed McDonald's and would not cause significant circulation problems in this area. The project site plan shows that 12 vehicles can be accommodated within the proposed drive-through storage area.

■ ■ ■

REFERENCES AND PERSONS CONTACTED

Associated Transportation Engineers

Richard L. Pool, P.E. Principal Engineer
Darryl F. Nelson, PTP, Senior Transportation Planner
Matthew Farrington, Transportation Planner

Persons Contacted

Maria Ester Rodriguez, City of Watsonville
Keith Boyle, City of Watsonville

References

Highway Capacity Manual, Transportation Research Board, National Research Council, 2010.

Trip Generation, Institute of Transportation Engineers, 9th Edition, 2013.

Draft Watsonville Vista 2030 General Plan, City of Watsonville, August 2012.

NOTICE OF EXEMPTION

Supplementary Document Q

TO: Office of Planning and Research
1400 Tenth St. Room 121
Sacramento, Calif. 95814

FROM: City of Watsonville
Community Development Dept.
250 Main Street
Watsonville, Calif. 95077-5000

XX Clerk of the Board
Santa Cruz County
701 Ocean St., Room 500
Santa Cruz, CA 95060

FILE NO. : PP2014-117, 175

Project Title: McDonald's

Project Location - Specific: 174, 186, & 190 Main Street (APN'S: 017-183-04, 05 & 10)

Project Location - City: Watsonville **Project Location-County:** Santa Cruz

Description of Project: Application (PP2014-175) for a Rezoning from Institutional Zoning (N) to Central Commercial (CC) for parcel (A.P.N. 017-183-04) with consideration of a Text Amendment to (WMC 14-16.1104 (b)), for allowance of drive-through facilities with a Use Permit in the CC Zone. Additionally, consider recommendation on Application (PP2014-117) for a Special Use Permit with Design Review and Environmental Review, to construct a 4,278 sq. ft. 24 hour fast food restaurant with a drive-through facility (McDonalds) at 174 and 186 Main Street; including a Variance to reduce the distance of the drive-through facility from 200 feet to 160 feet (WMC 14.41.100 (a) (2)) from a signalized intersection.

Name of Public Agency Approving Project: City of Watsonville

Name of Person or Agency Carrying Out Project: Core States Group, Allison Hazen

Exempt Status: (Check One):

- Ministerial (Sec.21080(b)(1); 15268);
- Declared Emergency (Sec. 21080 (b)(3);15269(a));
- Emergency Project (Sec. 21080 (b)(4); 15269(b)(c));
- Categorical Exemption. State Type and Section number: 15303**
- Statutory Exemptions. State code number:

Reasons why project is Exempt: New Commercial Structure less than 10,000 sq. ft. allowed per CEQA guidelines without identified environmental impacts

Lead Agency Contact Person: Keith Boyle, Principal Planner Telephone: 831-768-3073

If filed by applicant:

1. Attach certified document of exemption finding.
2. Has a notice of exemption been filed by the public agency approving the project? Yes _____ No

Signature: _____ **Date:** September 3, 2014 **Title:** Principal Planner

____ Signed by Lead Agency Date received for filing at OPR:
____ Signed by Applicant

RESOLUTION NO. ____-14 (PC)

RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF WATSONVILLE, CALIFORNIA, RECOMMENDING APPROVAL TO THE CITY COUNCIL OF (PP2014-175) TEXT AMENDMENT TO SECTION 14-16.1104 (B) OF THE WATSONVILLE MUNICIPAL CODE (WMC) ALLOWING DRIVE-THROUGH FACILITIES IN THE CC DISTRICT WITH A USE PERMIT, A ZONING MAP AMENDMENT CHANGING THE ZONING OF PARCEL 017-183-04 FROM INSTITUTIONAL (N) TO CENTRAL COMMERCIAL (CC), AND ALSO RECOMMENDING SPECIAL USE AND DESIGN REVIEW PERMIT WITH ENVIRONMENTAL REVIEW (PP2014-117) TO ALLOW CONSTRUCTION OF A 4,278 SQ. FT. 24 HOUR MCDONALD'S RESTAURANT; INCLUDING A VARIANCE TO REDUCE THE DISTANCE FOR A DRIVE-THROUGH FACILITY FROM 200 FEET TO 160 FEET FROM A SIGNALIZED INTERSECTION IN A CC ZONING DISTRICT.

Project: 174, 186, & 190 MAIN STREET (APN'S: 017-183-04, 05 & 10)

WHEREAS, a zoning text amendment and map amendment, Special Use and Design Permit, with Environmental Review and Variance (PP2014-117 & 175), to allow the construction of a 4,278 sq. ft. McDonald's restaurant at 174, 186 and 190 Main Street, Watsonville, California, was filed by Allison Hazen, Core States Group, applicant, on behalf of the owners, Fred Oda, Lupe Beltran, and the City of Watsonville on May, 15, 2014; and

WHEREAS, following the recommended Ordinance modifications, the project will be consistent with the provisions of the Zoning Ordinance of the City of Watsonville (Chapter 14 of the WMC); and

WHEREAS, the Planning Commission determined after reviewing the traffic study and historic study for the project, determined that the 4,278 sq. ft. building was allowed to be considered with a Class 15303 CEQA Exemption; and

WHEREAS, the Planning Commission has also considered Special Use Permit, Design Review and Variance application for the creation a 4,278 sq. ft. building on the site with a variance for a drive-through facility within 160 feet of a signalized intersection for a McDonald's fast food restaurant; and

WHEREAS, notice of time and place of the hearing to consider recommendation of approval of the text amendment, map amendment, Special Use and Design Review Permit and Variance with

Environmental Review (PP2014-117, & PP2014-175) was given at the time and in the manner prescribed by the Zoning Ordinance of the City of Watsonville. The matter called for hearing evidence both oral and documentary introduced and received, and the matter submitted for decision; and

WHEREAS, the Planning Commission has considered all written and verbal evidence regarding this application at the public hearing and has made Findings, attached hereto and marked as Exhibit "A", for the text amendment allowing drive-through facilities in the CC zone with a Use Permit, and Findings, attached hereto and marked as Exhibit "B", for the map amendment for parcel 017-183-04 from Institutional (N) to Central Commercial (CC) (PP2014-175) and further considered Findings, attached hereto and marked as Exhibits "C" & "D", in support of Special Use and Design Review Permit and Findings, attached hereto and marked as Exhibit "E", for the Variance with Environmental Review (PP2014-117) and Conditions, attached hereto and marked as Exhibit "F", to allow construction of a 4,278 sq. ft. restaurant with drive-through facility for McDonald's at 174,186, and 190 Main Street Watsonville, California.

NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Watsonville, California, as Follows:

Good cause appearing, therefore, the Planning Commission of the City of Watsonville does recommend approval to the City Council of the text amendment allowing drive-through facilities in the CC zone with a Use Permit, the map amendment for parcel 017-183-04 from Institutional (N) to Central Commercial (CC) (PP2014-175), the Special Use and Design Review Permit with Environmental Review (PP2014-165), attached hereto and marked as Exhibit "G", subject to the Conditions, attached hereto and marked as Exhibit "F", for construction of a 4,278 sq. ft. 24 hour restaurant with drive-through facility for McDonald's at 174,186, and 190 Main Street, Watsonville, California.

I HEREBY CERTIFY that the foregoing Resolution was introduced at a regular meeting of the Planning Commission of the City of Watsonville, California, held on the 2nd day of September, 2014,

by Commissioner _____, who moved its adoption, which motion being duly seconded by Commissioner _____, was upon roll call, carried and the resolution adopted by the following vote:

Ayes: Commissioners:

Noes: Commissioners:

Absent: Commissioners:

Marcela Tavantzis, Secretary
Planning Commission

Marty Corley, Chairperson
Planning Commission

**CITY OF WATSONVILLE
TEXT AMENDMENT**

EXHIBIT A

Application No. PP2014-175
APN: 017-183-04, 05 & 10
Address: 174, 186 & 190 Main Street
Applicant: Allison Hazen, Core States Group
Approval Date: September 2, 2014

Following is the proposed text amendment to clarify that Drive-Through Facilities are allowed in the CC zone with a Special Use Permit:

Part 11: CC Central Commercial District

14-16.1104 (b) the following require a Special Use Permit:

GLU

58 Eating and drinking places offering live entertainment or dancing

~~Exclusion: Drive-through facilities not allowed~~

Findings for the proposed Text Amendment

A. That the proposed amendment is consistent with the policies in the General Plan.

Supportive Evidence

The proposed text amendment allows new drive-through facilities in the CC zone to be consistent with several other existing restaurants and pharmacies in the zone. The modification will allow the development of projects surrounding the core of downtown to include facilities that will help promote the development of new facilities to encourage entry into the downtown. Projects can be reviewed on a case by case basis to determine that a drive-through is designed in a manner that provides safe ingress and egress to the site.

B. That the proposed amendment is compatible to the extent possible with the actual and General planned use of the adjacent properties.

Supportive Evidence

The proposed text amendment supports a new restaurant directly adjacent to two existing restaurants in the CC zone that have drive-through facilities. The allowance of a new drive-through facility will be reviewed on a case by case basis with a Use Permit to determine that the proposed facility will be consistent with uses on adjacent and nearby properties. Safe ingress and egress and compatibility with the drive-through standards will be determine as part of Use Permit review by the Planning Commission.

EXHIBIT A
Page 1 of 2

Central Commercial Zoning



City of
Watsonville

Motto: "Opportunity Through Diversity, Unity Through Cooperation."

Legend

- Parcels
- Watsonville City Limit
- PD: Planned Development
- Zoning**
- R-1P: Planned Single Family Residential
- R-1: Single Family Residential-Low Density
- RM-2: Multiple Residential-Medium Density
- RM-3: Multiple Residential-High Density
- CC: Central Commercial
- CCA: Central Commercial Core Area
- CN: Neighborhood Commercial
- CNS: Neighborhood Shopping Center
- CO: Office
- CT: Thoroughfare Commercial
- CV: Visitor Commercial
- IG: General Industrial
- IP: Industrial Park
- N: Institutional
- PF: Public Facilities
- EM-OS: Environmental Mgmt.-Open Space
- CZ-A: Coastal Zone-A
- CZ-B: Coastal Zone-B
- CZ-C: Coastal Zone-C
- CZ-D: Landfill
- CZ-E: Coastal Zone-E

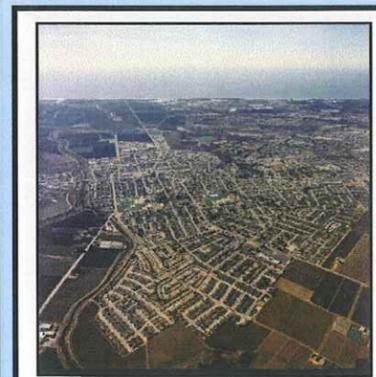
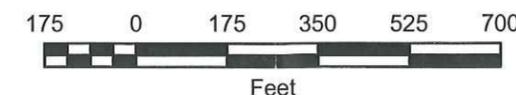
Prepared by Watsonville GIS Center 8/13/2014 (CDD1454).
This document is a graphic representation using the best currently available sources. The City of Watsonville assumes no responsibility for any errors.

EXHIBIT **A**

Page **2** of **2**



1 inch = 350 feet



**CITY OF WATSONVILLE
MAP AMENDMENT**

EXHIBIT B

Application No. PP2014-175

APN: 017-183-04

Address: 190 Main Street

Applicant: Allison Hazen, Core States Group

Approval Date: September 2, 2014

The project proposes to rezone the surplus City parking facility from Institutional (N) to Commercial Core (CC) to allow the sale of the site to the developers for the development of a McDonald's Restaurant.

Findings for the proposed Map Amendment

A. That the proposed amendment is consistent with the policies in the General Plan.

Supportive Evidence

The proposed map amendment allows the surplus City parking to be sold to the developer for the development of a McDonald's restaurant in the CC zone. The map amendment is consistent with the underlying 2030 General Plan designation. Following rezoning, the parcel will be merged with the adjacent parcels that are also zoned CC to maintain consistent zoning across the site.

B. That the proposed amendment is compatible to the extent possible with the actual and General planned use of the adjacent properties.

Supportive Evidence

The proposed map amendment supports a new restaurant directly adjacent to two existing restaurants in the CC zone. The modification of the map will allow the sale of the surplus City parking lot for the development of a restaurant that is consistent with similar uses in the area. Use of the existing driveways will allow circulation in the area to remain the same and the site will continue to be used as parking and circulation for private access to the restaurant. The proposed map amendment allows the parcel to be merged with the adjacent parcels and maintain consistent zoning on the entire parcel.

EXHIBIT B
Page 1 of 2

Proposed Re-Zoning from N to CC



City of Watsonville

Motto: "Opportunity Through Diversity, Unity Through Cooperation."

Legend

- ☐ Parcels
- 📍 Watsonville City Limit
- Zoning**
- R-1P: Planned Single Family Residential
- R-1: Single Family Residential-Low Density
- RM-2: Multiple Residential-Medium Density
- RM-3: Multiple Residential-High Density
- CC: Central Commercial
- CCA: Central Commercial Core Area
- CN: Neighborhood Commercial
- CNS: Neighborhood Shopping Center
- CO: Office
- CT: Thoroughfare Commercial
- CV: Visitor Commercial
- IG: General Industrial
- IP: Industrial Park
- N: Institutional
- PF: Public Facilities
- EM-OS: Environmental Mgmt.-Open Space
- CZ-A: Coastal Zone-A
- CZ-B: Coastal Zone-B
- CZ-C: Coastal Zone-C
- CZ-D: Landfill
- CZ-E: Coastal Zone-E

Prepared by Watsonville GIS Center 8/13/2014 (C0DD1454).
 This Document is a graphic representation using the best currently available sources.
 The City of Watsonville assumes no responsibility for any errors.



1 inch = 100 feet

25 0 25 50 75 100



Feet

APN: 017-183-04

EXHIBIT

B

Page 2 of 2

Application No. PP2014-117
APN: 017-183-04, 05 & 10
Address: 174, 186 & 190 Main Street
Applicant: Allison Hazen, Core States Group
Approval Date: September 2, 2014

DESIGN / SPECIAL USE PERMIT FINDINGS (Section 14-12.403, 513)

1. The proposed use at the specified location is consistent with the policies of the General Plan and the general purpose and intent of the applicable district regulations.

Supportive Evidence

With the proposed text modifications to the CC zone relating to drive-through facilities, the proposed project is consistent with the CC (Central Commercial) zoning district regulations and meets all of the requirements of the Zoning Ordinance for the site, with the approval of a Special Use Permit.

2. The proposed use is compatible with, and preserves the character and integrity of adjacent development and neighborhoods, and includes improvements or modifications either onsite or within the public rights-of-way to mitigate development-related adverse impacts such as traffic, noise, odors, visual nuisances, or other similar adverse effects to adjacent development and neighborhoods. These improvements or modifications may include but shall not be limited to the placement or orientation of buildings and entryways, parking areas, buffer yards, and addition of landscaping, walls, or both, to mitigate such impacts.

Supportive Evidence

General architectural considerations are consistent with all design standards including materials, colors, and general design. The structure is consistent with surrounding service station and two other restaurants at the entrance to Main Street and the modern building design will complement the entry gateway to Main Street.

3. The proposed use will not generate pedestrian or vehicular traffic that will be hazardous or conflict with the existing and anticipated traffic in the neighborhood.

Supportive Evidence

The project will utilize the existing driveways that have been utilized for many years. These driveways will be expanded and sidewalks designed to meet ADA standards. Internal circulation has been designed to separate the drive through traffic from the internal circulation and it will not create traffic conflicts either on or off site.

4. The proposed project incorporates roadway improvements, traffic control devices or mechanisms, or access restrictions to control traffic flow or divert traffic as needed to reduce or eliminate development impacts on surrounding neighborhood streets.

Supportive Evidence

The project will maintain the existing circulation facilities that will not be impacted by the new building. The traffic study indicates that the new facility will not generate additional traffic significant enough to create new traffic conflicts.

- 5. The proposed use incorporates features to minimize adverse effects, including visual impacts and noise of the proposed special use on adjacent properties.

Supportive Evidence

The proposed project provides new landscaping that will be maintained as part of the project. A landscape plan will be provided as part of the building permit application that will include low impact development standards to ensure compliance with Region water quality control standards.

- 6. The proposed special use complies with all additional standards imposed on it by the particular provisions of this chapter and all other requirements of this title applicable to the proposed special use and uses within the applicable base-zoning district.

Supportive Evidence

Following text amendments to the CC zone for drive-through facilities, the proposed project is consistent with the CC (Central Commercial) zoning district regulations and meets all of the requirements of the Zoning Ordinance for the site.

- 7. The proposed special use will not be materially detrimental to the public health, safety, convenience and welfare, and will not result in material damage or prejudice to other property in the vicinity.

Supportive Evidence

The project as designed will not be detrimental to public health, safety, convenience or welfare, and will not damage other property in the vicinity.

**CITY OF WATSONVILLE
VARIANCE**

EXHIBIT E

Application No. PP2014-117
APN: 017-183-04, 05 & 10
Address: 174, 186 & 190 Main Street
Applicant: Allison Hazen, Core States Group
Approval Date: September 2, 2014

Variance Findings (Section 14-12.603 (b))

1. By reason of narrowness shallowness, or shape of the property in question, or by reason of exceptional topographic conditions or other extraordinary and exceptional situation or condition of the property in question, the strict application of the provisions of this title would result in a peculiar and exceptional practical difficulties to, or exceptional and undue hardship to the applicant.

Substantial Evidence

The Drive-Through Ordinance intends that drive-through facilities be provided a safe distance from signalized intersections to ensure adequate movement on and off the site. The proposed driveway that serves the facility is in the same location as the existing driveway for the current surplus City parking lot. This drive-way has been used for many years and has not created traffic safety hazards. The existing buildings that block view of the driveway will be demolished and the new building will provide significant visual distance at the exit in accordance with the drive-through standards. Since the project is enhancing an existing driveway and all circulation is being maintained on site, the existing drive-through regulations appear to provide an undue hardship because the project can be designed to meet safety requirements.

2. The circumstances or conditions do not apply generally to other properties in the same land use district.

Substantial Evidence

There are two other restaurants in the CC zone adjacent to this parcel, that have drive-through entrances that are significantly closer than the proposed facilities. This variance will be consistent with adjacent properties.

3. The granting of the variance will not result in material damage or prejudice to other properties in the vicinity, substantial impairment of natural resources or be detrimental to the public health, safety and general welfare.

Substantial Evidence

A variance for the location of the drive-through facility will not impair the use of the other facilities in the area. The use of the existing driveway and better visual access to the site created by demolishing the existing building and replacing with a smaller building will increase safety on the

site. Granting of the variance will not be detrimental to the health and safety of the area but provide a new improved facility that will complement the entry to downtown.

4. The variance is not requested exclusively on the basis of economic hardship to the applicant, nor was the situation requiring a Variance created by an act of the applicant without obtaining permits.

Substantial Evidence

The variance is being requested because the restaurant is proposing to use a driveway access that has been in existence for many years. This access will be enhanced to allow greater circulation movement on the site and provide more visibility to enhance safe vehicle movement. This is not a hardship created by the applicant. The City is interested in promoting a new family friendly facility at the downtown's entrance that will be achieved by the proposed project.

EXHIBIT E
Page 2 of 2

**CITY OF WATSONVILLE
SPECIAL USE/DESIGN REVIEW PERMIT**

EXHIBIT F

Application No.: PP2014-117

APN: 017-183-04, 05 & 10

Location: 174,186 & 190 Main Street

Applicant: Allison Hazen, Core States Group.

Hearing Date: September 2, 2014

CONDITIONS OF APPROVAL:

General Conditions Design Review:

1. This Design Review shall be null and void if not acted upon within 24 months from the effective date of the approval thereof. Time extensions may be granted provided the applicant requests same at least thirty (30) days in advance of the expiration of this Design Review Permit. This approval applied to the plans titled "Auto Zone" received by the Community Development Department on July 14, 2014. Extensions shall be consistent with that outline in the Specific Plan(CDD-P)
2. After approval is granted, modifications to the project or to conditions imposed may be considered in accordance with Chapter 14-12.1000 of the Watsonville Municipal Code. (CDD-P)
3. Approval is subject to making findings and supportive evidence in accordance with Section 14-12.402 of the City Zoning Ordinance, with attached said Findings, and made a part of this Special Use/Design Review Permit. (CDD-P)
4. This approval shall be effective fourteen (14) days after the date of approval. In the event of an appeal, the effective date of this approval shall be withheld until after the final determination thereof by the Planning Commission. (CDD-P)
5. The project shall be in compliance with the conditions of approval, all local codes and ordinances, appropriate development standards and current City policies. Any deviation will be grounds for review by the City and may possibly result in revocation of the Design Review Permit or other Code Enforcement Action. (CDD-P)
6. A copy of the final conditions of approval must be printed on the front sheet of plans submitted for future permits. **Plans without the conditions of approval printed directly on the front page will not be accepted at the plan check phase.** (CDD-P)

Prior to or concurrent with the submittal of a Building Permit application, revise plans to show the following:

7. Unless otherwise noted, City of Watsonville Public Improvement Standards shall be used for private as well as public improvements. All development shall comply with the City of Watsonville Public Improvement Standards. Plans and design documents shall be signed and stamped by a California Licensed Architect or Engineer. Standards that are different than

those of the City must be approved by the City. City Standards shall be included in the plans. (CDD-E)

8. An Erosion Control Plan shall be submitted with the permit application. Erosion control plans shall provide Best Management Practices (BMPs) during construction to prevent erosion of constructed slopes, and sediment and contaminants from being entrained in runoff. BMPs shall comply with the City of Watsonville Erosion Control Standards and the Erosion and Sediment Control Field Manual by the California Regional Water Quality Control Board, San Francisco Region, latest edition. All erosion control shall be installed prior to October 15 and be maintained in place at least until April 15. The applicant shall ensure that all contractors are aware of all erosion control standards and BMPs. (CDD-E)
9. The project applicant shall execute an agreement in the standard form providing for the maintenance, and monitoring and reporting of those activities to the City of Watsonville storm drain systems best management practice measures. (CDD-E)
10. Project shall comply with the City's Storm Water Management Plan and NPDES Best Management Practices. Project shall develop storm water management improvements in substantial conformance with conceptual plans. In order to prevent a significant impact occurring, the project drainage must be designed to meet City Best Management practices, NPDES standards, public works drainage standards, and Low Impact Development (LID) standards. (CDD-E)
11. Prior to permit issuance, applicant shall execute an Engineering Testing and Inspection Agreement and submit to the City for approval. Applicant shall hire a testing firm to perform engineering testing and inspection, such as soils and concrete testing and inspection. Testing firms shall be organized, directed and under the supervision of a registered engineer. The testing and inspection shall be done at the direction of the City Inspector. The testing firm shall report nonconforming items to the City Inspector and furnish daily, weekly and final reports as outlined in the agreement and directed by the City Inspector. (CDD-E)
12. Submit a report detailing a comprehensive investigation of surface and subsurface soil and geotechnical conditions prepared by a registered civil or geotechnical engineer. The report and investigation shall address seismic hazards. The report shall determine soil properties and strengths to allow for design of retaining walls, foundations and pavements. In addition, the soils report shall determine the permeability of on-site soils. (CDD-E)
13. On the plans, show the location of proposed mailboxes. Provide written approval of mailbox locations from the US Postal Service. (CDD-P, E)
14. Separate On/Off Site Permits are required for work in the public right-of-way. (CDD-E)
15. Trash enclosures shall be located and designed subject to the review and approval of the City of Watsonville. The City of Watsonville provides front load service for dumpsters and rear load service for drop boxes and compactors. Enclosures shall be sized to accommodate the number of refuse and recycling containers required to store refuse and recycling generation for one week. The design of trash enclosures shall follow, in general, City Standard Drawings Nos. S-809A, S-809B, S-809C and S-810. (CDD-E)

EXHIBIT F
Page 2 of 8

16. Provide landscape and irrigation plans indicating types, quantities, locations and sizes of all plant material including existing major vegetation designated to remain, street trees, method of protecting planting areas from vehicular traffic. Landscape areas shall be incorporated into storm drainage best management practice measures and therefore the plant materials selected must include appropriate ground covers which filter sediment and pollutants and be tolerant of occasional inundation. All landscaping and irrigation shall be installed and approved prior to occupancy of the project. (CDD-E, P)
17. Three copies of the final landscape and irrigation plans must be submitted and approved by the Community Development Department. The landscape plans shall be coordinated with any bioswales throughout the project. The project shall utilize drought tolerant species, street trees, planter areas, common areas and public right-of-ways. All trees shall be a minimum size of 15 gallons with a minimum spread of four-to-five feet, and one inch caliper trunks. All of the street trees, and trees along the private driveway, shall be 24-inch box specimens. A minimum of 25% of the shrub material shall be a minimum five-gallon size. Automatic, low-flow irrigation systems shall be installed in all landscaped areas. Irrigation is to be programmed for night or early morning hours in order to minimize evaporation. The landscaping plan for all public areas shall be reviewed and approved by the Community and Parks and Recreation Departments for conformance with the Development Plans and this Condition. Additional buffer trees shall be installed along the rear property line adjacent to the Sea View Ranch retaining wall.(CDD-P, PK)
18. In order to achieve the maximum extent possible standard of minimizing hydro-modification, landscape and open-space areas shall be incorporated into the storm drain best management practice measures by means of Low Impact Design. Landscape areas shall be designed with appropriate ground cover to filter pollutants and prevent erosion, and be graded to promote low runoff velocities and enhance absorption into surrounding soils. (CDD-E)
19. All development shall utilize water conservation, water recycling, and xeriscaping to the maximum extent possible. Irrigation systems shall be designed and maintained to avoid run-off, over-spray, or other similar conditions where water flows to waste. Turf shall not be used in median strips, parking islands, or in areas less than eight (8) feet wide, or on slopes that will result in excess irrigation water run-off. (CDD-P, E)
20. In order to reduce the impact of soil erosion or the loss of topsoil to a less than significant impact, the finished ground surface of the project will be planted with ground cover and continually maintained to minimize surface erosion. Final grading and landscaping must not obstruct the site drainage or allow moisture to accumulate adjacent to foundations, slabs, pavements, or other improvements. (CDD-E, CDD-P)
21. The locations of surface mounted utility facilities such as pedestals, transformers backflow devices and fire services shall be planned so that they may be screened utilizing landscaping or other acceptable, visually pleasing means subject to the review and approval of the City of Watsonville. (CDD-E, P)
22. Prior to issuance of a building permit or the commencement of any site work, the project applicant and the general contractor shall attend a pre-construction meeting with the Building Official and City staff to discuss the project conditions of approval, working hours, site maintenance and other construction matters. The general contractor shall acknowledge that he/she has read and understands the project conditions of approval, particularly those

pertaining to construction practices and site safety, and will make certain that all project sub-contractors have read and understand them prior to commencing work and that a copy of the project conditions of approval will be posted on site at all times during construction. (CDD-B, P, E)

23. Electric and communications services to new buildings shall be constructed underground. Aerial services are prohibited. (CDD-E)
24. The project will be required to pay Traffic Impact Fees. Fees shall be collected in conformance with standard WMC fee ordinance (CDD-E)
25. Parking lot layout and parking stall geometrics shall conform to City Standard Plan S-808a. Sidewalks adjacent to angled parking rows shall be sufficiently wide to allow disabled pedestrian access around overhanging cars. Drive aisles shall meet standards for diagonal parking. (CDD-E)
26. No permanent improvements shall be permitted over easements without written authorization from the easement holder. (CDD-E)
27. Obtain an encroachment permit for new street improvements driveways and other work in the public right-of-way. The applicant shall be responsible for any repairs within the limits of the development, including streets and paving, curbs and gutters, sidewalks, and street lights, or installation of same where not existing. (CDD-E)
28. Plans shall include the location and size of all building utility service connections, including water, gas, electric, fire and irrigation services. Plans shall indicate water service/s size and location and sewer service/s size, type, and slope. Connections shall be located, sized and screened in such a manner that they have the least possible impact on the design of the building and site. (CDD-E)
29. Project shall comply with the underground utility provisions of Watsonville Municipal Code Title 7, Chapter 16. (CDD-E)
30. The project shall connect to the local sewer with a 6x10 wye on the 10" Main. (CDD-E, PW)
31. Water services will require City-approved backflow prevention devices. Backflow prevention devices shall be located within 5 feet of the water meters and shall be adequately screened. (CDD-E, P)
32. The existing Sanitary Sewer laterals from 174, 186 and 190 Main Street will need to be abandoned sufficiently to prevent inflow and infiltration of groundwater and/rain water from entering the Sanitary Sewer. (PW)
33. Maintain existing monitoring wells. (PW)
34. Solid waste generated during the construction shall be serviced by the City of Watsonville Solid Waste Division. Applicant shall submit a Solid Waste Service Plan on the City form. (CDD-E, PW)

EXHIBIT

Page 9 of 8

35. In order to reduce the impact of strong seismic ground shaking to a less than significant impact the project shall comply with the 2013 building code as it relates to seismic design and the recommendations of the Geotechnical report. (CDD-E, CDD-B)
36. Improvements (new framing, electrical, mechanical, plumbing) Obtain all required building permits (Building, Plumbing, Mechanical, Grading etc.) for this project. All construction shall comply with all State Building Codes; Framing, mechanical, plumbing, electrical, T-24 energy, T-24 Accessibility and Municipal codes in effect at the time of plan submittal for building permits resulting in actual construction. (CDD-B)
- a. A design professional will be required at time of construction drawings, to prepare plans for proposed improvements per the business and professions code.
 - b. Comprehensive detailed construction plans are required at the time of submittal to be reviewed for adequate content prior to intake, by the Building Official.
 - c. Provide Occupancy group type of construction proposed per the CBC, to clearly determine requirements.
 - d. In January 1, 2013 the new California Building Code (CBC) became effective. All construction projects shall comply with the City's Green Building Ordinance and all amended California Building Codes including:
 - 2013 California Building Code
 - 2013 California Electrical Code
 - 2013 California Mechanical Code
 - 2013 California Plumbing Code
 - 2013 California Energy Code
 - 2013 California Fire Code
37. Prior to excavation, adjoining landowners shall be given notice of the date, location, and extent of excavation in conformity with Section 832 of the Civil Code and copies shall be provided to the Building Official prior to issuance of the Building Permit. (CDD-B)
38. Prior to requesting a Building Department foundation inspection, the soils engineer shall inspect and approve the mitigation measures and the foundation excavations. The soils engineer shall submit documentation to the Building Division which verifies compliance with the recommendations specified in the soils report. (CDD-B)
39. As indicated by CBC Section 1704, the owner or the engineer or architect of record acting as the owner's agent shall employ one or more special inspectors who shall provide special inspections when required by CBC Section 1704. Please contact the Building Division at time of plan submittal to obtain application for special inspections. (CDD-B)
40. Provide Structural Calculations verifying compliance with all applicable provisions of the California Uniform Building code Chapter 16. Prior to request for final inspection, written verification by the engineer of record indicating compliance with the structural design shall be submitted to the City of Watsonville Building Division. (CDD-B)
41. Comply with all applicable provisions of the California State Building Code (Title 24) Part 2 Chapter 11B Division I, II & III for Disabled Access. Plans must show compliance in sufficient information and detail to determine compliance was noted for the following:
- a. Path of travel from Public Transportation (main entry to the public sidewalk).

- b. Disabled parking requirements:
 1. Van Accessible Parking (requires 8'-0" unloading area)
 2. Number of spaces (1 for 1st 25, 2 for the next 50 see table 11B-6).
 3. Path of travel from accessible parking to any elevators
 4. Slopes at parking & unloading areas must not exceed 1:50
 5. Proper disabled signage, lettering and stripping is required (CDD-B)
42. Main building entrances and required exits must be accessible. Design professional must provide written verification of compliance for existing disabled access features or facilities noted on plans. (CDD-B)
43. The building shall have Automatic Fire Sprinklers installed, complying with NFPA installation standards. The fire sprinkler contractor shall submit three sets of plans and calculations for a separate fire permit prior to installation of the system. (CDD-B, WFD)
44. The project shall comply with the current CBC, and CFC 2013 regulations for fire issues. (CDD-B, WFD)
45. A UL central station shall monitor all fire sprinkler systems. The monitoring shall provide water flow notification to the hearing and visually impaired. The monitoring contractor shall submit three sets of plans for a separate fire permit prior to installation of the system. (CDD-B, WFD)
46. The building shall be provided with the required size and number of fire extinguishers. Exterior doors providing access to the fire risers and alarm panel shall have proper signage installed. (CDD-B, WFD)
47. Civil drawings shall include the proposed location of the fire department connection, above ground backflow device, post indicator valve, and size of fire service needed for this project (CDD-B, WFD).
48. Adequate water for firefighting shall be available prior to combustible construction. (CDD-B, WFD)
49. Separate application submittals will be required for kitchen's fixed fire suppression systems, and monitoring systems. (CDD-B,WFD)
50. In order to prevent an adverse impact from exterior light sources, a final lighting plan will be required to be submitted at the time of building permit application to ensure that the location, height, and angle of all exterior lighting will not interfere with adjacent properties including the neighboring airport. (CDD-P)
51. Restaurant shall provide a grease trap system approved by the Public Works Solid Waste Division (PW)
52. A plan for tallow storage is required. It may not be stored in the trash enclosure. (PW)
53. Provide a detailed color and Materials Board. A copy of the paint colors for the project with indication of location of each color (color board), shall be submitted for review and approval by the Community Development Department. (CDD-P)

54. The parcels shall be merged before final occupancy is granted (CDD-B)
55. The front elevation of the restaurant along Main Street shall be updated to include a stone façade (CDD-P)

During construction, the following conditions shall be adhered to:

General Conditions:

56. Provide Best Management Practices (BMPs) during construction to prevent sediment, debris and contaminants from draining offsite. BMPs shall comply with the City of Watsonville Erosion Control Standards and the Erosion and Sediment Control Field Manual by the California Regional Water Quality Control Board, San Francisco Region, latest edition. All erosion control shall be installed prior to October 15 and be maintained in place until April 15. Provide a note on the improvement plans stating that construction should take place between April 15 and October 15. The applicant shall ensure that all contractors are aware of all erosion control standards and BMPs. (CDD-E)
57. The project applicants shall construct a trash enclosure and separate storage facility on the Police lot and provide a screening fence along the boundary of the Police lot in accordance with the Police Department requirements. (CDD-B)

Construction Notes to be included with the Improvement Plans:

58. Existing public facilities damaged during the course of construction or in an existing state of disrepair shall be repaired by the applicant, at the applicant's expense, to the satisfaction of the City. (CDD-E)
59. Applicant shall have onsite at all times, a superintendent that shall act as the owners' representative and as a point of contact for the City's Public Works Inspector. The superintendent shall be authorized by the Owner to direct the work of all contractors doing work on public and private improvements. (CDD-E, PW)
60. Contractor shall provide a minimum of 48 hours' notice in advance of any required inspection. Any temporary suspension of work or returning to work for any reason shall be cause for the developer or contractor to telephone the Public Works Inspector at 768-3100. (CDD-E)
61. Prior to excavation, contractor shall locate all existing underground utilities. Call Underground Service Alert (U.S.A.) at 1-800-642-2444 to have utilities located and marked in the field. (CDD-E)

Prior to Final Occupancy:

62. Prior to issuance of the certificate of occupancy, the project designers including civil, structural, and geotechnical engineers shall provide statements of compliance attesting that they have reviewed the completed project and that it was constructed in conformance with their recommendations and plans. (CDD-E)
63. As-built plans shall be provided for the project in the following manner:

- a. The applicant's contractor shall maintain one set of full size, approved plans and mark thereon any deviations from plan dimensions, elevations or orientations. Marked plans shall be updated weekly and shall be available to the City for review when requested. Revisions to the plans shall be done in black ink. They shall be clouded and a delta or a note placed next to the clouding that indicates that the change was done as the plans were being "as-built." As-built plans shall be maintained for all approved improvement plans, including but not limited to, grading, retaining wall, drainage, utility, roadway, landscape and irrigation plans.
64. Prior to final City acceptance of the project, all design professionals who prepared improvement plans for the project (civil, geotechnical, electrical and structural engineers), shall provide letters attesting that they have periodically monitored the construction and have reviewed the completed work and that it was constructed in substantial conformance with their plans and recommendations. Where special inspections and testing were involved, the letters of compliance shall be accompanied by inspection logs, testing and analysis that support the engineer's conclusions. (CDD-E)

Ongoing Conditions:

- 65. Landscaping and site improvements shall be maintained in perpetuity. Landscaping shall be maintained by a professional landscape maintenance company. All dying and/or diseased vegetation shall be immediately replaced in kind. (CDD-P)
- 66. No deliveries shall occur before 7:00 a.m. (CDD-P)

Key to Department Responsibilities

CDD-P	Community Development Department – Planning
CDD-B	Community Development Department – Building
CDD-E	Community Development Department – Engineering
PW	Public Works & Utilities Department
WFD	Watsonville Fire Department
CAT	City Attorney

EXHIBIT F
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**CITY OF WATSONVILLE
CITY COUNCIL**

EXHIBIT G

Application No. PP2014-117
APN: 017-183-04, 05 &10
Address: 174,186 & 190 Main Street
Applicant: Allison Hazen
Hearing Date: September 23, 2014

Applicant: Allison Hazen, Core States Group
Property Owners: Fred Oda, Lupe Beltran, & City of Watsonville
Address: 150 Lake Street, Suite 212, Kirkland WA 98119
Project: Text Amendment (14-16.1104) allowing drive-through facilities with a Use Permit in CC zone, Map Amendment changing zoning for parcel 017-183-04 from I to CC (PP2014-175), Special Use and Design Review Permit with Environmental Review & Variance (PP2014-117)
Location: 174,186 & 190 Main Street, Watsonville, CA 95076
Purpose: Special Use and Design Review Permit with Environmental Review to allow construction of a 4,278 sq. ft. 24 hour restaurant (McDonald's) with a Variance to allow the reduction in the driveway entrance from 200 feet to 160 feet from a signalized intersection for McDonald's.

The Text Amendment, Map Amendment, Special Use and Design Review Permit with Variance and Environmental Review Application (PP2014-117 & 175) requested by the applicant to allow the construction of a 4,278 sq. ft. 24 hour McDonald's restaurant with drive-through at 174, 186 and 190 Main Street, was reviewed by the City Council at a public hearing on September 23, 2014 and was conditionally approved by adoption of City Council Resolution No. _____ (CM) together with findings and conditions of approval attached hereto and made a part of this permit.

**CITY OF WATSONVILLE
Planning Commission**

Marcela Tavantzis
Community Development Director

EXHIBIT G
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